

# Land at Sodbury Road

Wickwar



Design and Access Statement

Rev B

December 2022

## Foreword

This revised Design and Access Statement (DAS) has been produced by Turley, on behalf of Bloor Homes, in support of updates to the outline planning application for Land at Sodbury Road, Wickwar (Ref: P22/01300/O), submitted to South Gloucestershire Council (SGC) in February 2022.

The Planning Application seeks consent for a new residential led development on this site. The description of development is as follows:

*Planning application for residential development of up to 180 new homes and associated infrastructure, including highways, drainage, publicly accessible open space and a new village shop.*

This revised DAS summarises the updated scheme following consultation with the Local Planning Authority, statutory consultees, and review by the Design West Design Review Panel.

This DAS should be read in conjunction with the Planning Statement, and associated technical reports which are in support of this application.

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# Introduction



# The Site

## Site Location

Wickwar is a village and civil parish in South Gloucestershire, England.

The village is approximately 6km north of Yate, 20km north east of Bristol and 32km south of Gloucester. All of the settlements can be accessed via Sodbury Road (B4060) which runs parallel to the site.

## Site Description

The site lies to the west of Sodbury Road (B4060) and is approximately 7.89 hectares in size. It constitutes four fields currently in agricultural use.

The site is bound by a mix of hedgerow, some trees or an informal boundary. Internal field boundaries are also defined by hedgerow and trees.

The northern boundary abuts South Farm and the associated agricultural buildings to the north east of the boundary.

The eastern boundary is bound partially to the east by Sodbury Road (B4060). Here the urban edge of the village adjoins the boundary, abutting rear gardens of residential properties that front onto Sodbury Road.

The southern boundary touches the junction at Frith Lane and follows an existing field boundary lines by trees and hedgerow with agricultural fields beyond.

The western boundary is lined by trees and hedgerow and abuts agricultural fields with open views into the wider landscape. There is an existing watercourse that lines the western boundary to the north.

The site has a varied topography ranging from approximately 90m to 83m Above Ordinance Datum (AOD) at its lowest point. The topography is generally flat although, there is a shallow depression which runs north to south within the site.

There are two approved planning applications for residential development (Pk16/4006/O and PK17/4552/O) located to the east of the site, on the opposite side of Sodbury Road. Both schemes have been granted approval and are currently under construction.

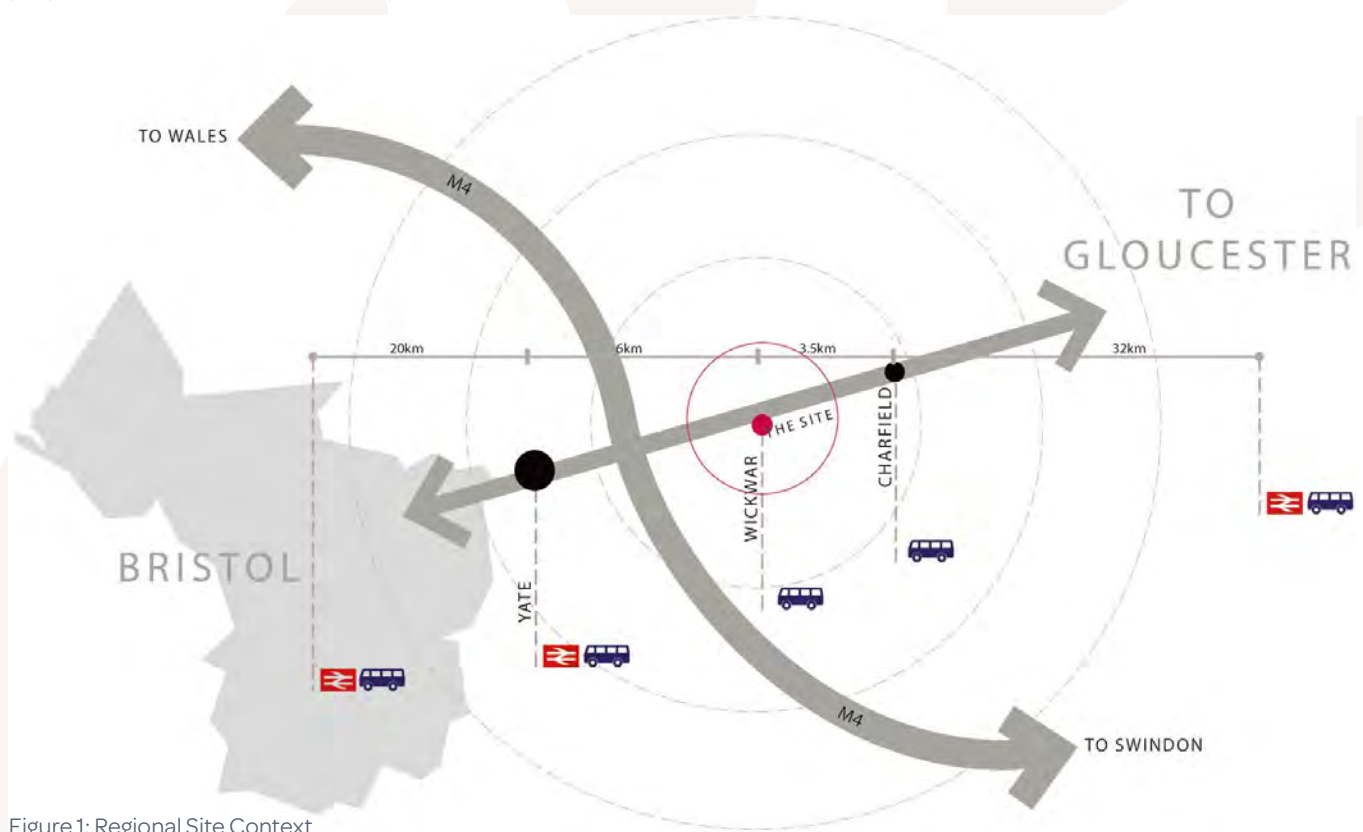


Figure 1: Regional Site Context





Figure 2: Site Location Plan





# Context





# Planning Policy Context

## Overview

The application site is located outside of the Green Belt and is not part of the AONB. Development at Wickwar will help to meet housing needs for South Gloucestershire. This application includes proposals for a convenience store, which would be a key benefit to the village as there is no current retail provision at Wickwar. Additional housing at the settlement will help to support further additional services and facilities, to the benefit of the overall sustainability of Wickwar as a place.

The relevant policy position and considerations in respect of the application are set out in detail in the Planning Statement, a short overview is provided below.

## The Development Plan

The current 'Development Plan' for the site consists of the South Gloucestershire Core Strategy (2006-2027) and the South Gloucestershire Policies, Sites and Places Plan.

Other material considerations include:

- The National Planning Policy Framework (March 2012) (NPPF) and National Planning Practice Guidance (NPPG);
- Supplementary Planning Documents; and
- New South Gloucestershire Local Plan.

Whilst the Development Plan is the starting point for making determinations under the Planning Acts, other up to date material considerations are significant.

## Core Strategy 2006-2027

The South Gloucestershire Core Strategy 2006-2027 was adopted on 11 December 2013. The strategy sets out the general location for development, its type and scale, as well as various other strategic policies to protect the physical and environmental assets of the area. The key policies relevant to the design of the proposals are summarised below.

Policy CS1, high quality design, states development will only be permitted where the highest possible standards of design and site planning are achieved. The Policy requires development to be of an appropriate scale and design and to be well integrated into the area.

Policy CS2, green infrastructure, states that it will be ensured that new green infrastructure is planned, delivered and managed as an integral part of creating sustainable communities and enhancing quality of life. The policy goes on to identify 7 objectives with which proposals should accord.

Policy CS16, housing density, states that housing development should make efficient use of land to conserve resources and maximise the amount of housing supplied. The Policy continues, stating new development should contribute to:

- The high quality design objectives set out in Policy CS1
- Improving the mix of housing types in the locality; and
- Providing adequate levels of public open spaces, semi-private communal open space and private outdoor space.

Policy CS17, housing diversity, states new housing development should comprise both market and affordable homes and provide a wide variety of housing types and provide a wide variety of housing type and size.

Policy CS18, affordable housing, seeks a provision of 35% affordable housing, unless the developer can demonstrate that the economic viability of the site is affected and as a result that a lower percentage should be provided. In this case other financial contributions will be sought.

Policy CS24, green infrastructure, sport and recreation standards, states that new development is required to comply with all local standards of provision in terms of quantity, quality and accessibility. Further to this, provision must be delivered on-site, unless it is demonstrated that partial or full off-site provision or enhancement creates a more acceptable proposal.

## The Policies, Sites and Places Plan

The Policies, Sites and Places plan, adopted 8 November 2017, replaces the remaining saved policies of the South Gloucestershire Local Plan 2006 and supplements the adopted Core Strategy by dealing with a range of more detailed Development Management matters.

Policies PSP1 and PSP2 relate to responding to climate change and high quality design. Policy PSP1, Local Distinctiveness, states that proposals will be acceptable where the proposals demonstrate an understanding of, and respond constructively to the buildings and characteristics that make a particularly positive contribution to the distinctiveness of the area / locality.

Policy PSP2 continues, stating that open space provision will be required to be of a high standard of design and appropriate to the use and character of the development and its location. It must be designed to be an integral part of the development but incorporating existing landscape features where appropriate, for the benefit of the proposal.

Policy PSP8 relates to managing future development, specifically regarding residential amenity. Policy PSP8 states that development proposals will be acceptable provided that they do not create unacceptable living conditions or have an unacceptable impact on the residential amenity of occupiers of the development or of nearby properties.

Policy PSP11 'Transport Impact Management' relates to managing travel demand generated by development. For residential development it states that residential development proposals are located on safe, usable walking and, or cycling routes, that are an appropriate distance to key services and facilities or an appropriate distance to a suitable bus stop facility, served by an appropriate public transport service(s), which connects to destination(s) containing the remaining key services and facilities.

Policy PSP43, private amenity space standards, states all new residential units will be expected to have access to private amenity space.

## Other Material Considerations:

### National Planning Policy Framework (NPPF)

The revised NPPF was published in July 2021 and sets out the Government's planning policies for England and how these are expected to be applied.

At the heart of the NPPF is a presumption in favour of sustainable development, which should be seen as a golden thread running through both plan-making and decision-taking. Sustainable development includes three elements – economic, social and environmental.

### National Planning Practice Guidance

The National Planning Practice Guidance (NPPG) was published in March 2014 and supports the NPPF and provides guidance on housing and economic development needs and availability assessments, the consideration and determination of planning applications, and various other technical and procedural matters, supporting the overall implementation of the policy contained within the NPPF.

### New South Gloucestershire Local Plan

Work has commenced on the New South Gloucestershire Local Plan, which will replace the Core Strategy and Policies, Sites and Places Plan and cover the period 2018-2036. A consultation was undertaken on initial draft documents (Issues and Approaches) in winter 2020/21. The next stage of consultation is due in Spring 2022.

The New Local Plan is currently at an early stage in the plan process and no draft policies relating to development at Wickwar ha



## National Design Guide

The National Design Guide, published in September 2019 by the Ministry of Housing, Communities and Local Government, seeks to illustrate how well-designed places that are beautiful, enduring and successful can be achieved in practice.

The guidance sets out 10 key characteristics of well-designed places which all work together to create character; help nurture and sustain a sense of community; and address environmental issues affecting the climate.

The National Design Guide, illustrates how well-designed places that are beautiful, enduring and successful can be achieved in practice. It forms part of the Government's collection of planning practice guidance and should be read alongside the separate planning practice guidance on design process and tools'.

The development at Wickwar has been designed to address ten characteristics highlighted on the NDG:

- **Context** - The context of Wickwar has been analysed to make sure that the design responds positively to its features, takes cues and inspiration of its surroundings and enhances its setting.
- **Identity** - the identity of the design has been informed by Wickwar and interpreted to create a unique one for the development.
- **Built form** - The development will deliver an appropriate mix of building types, forms and scale along with green open spaces to create a coherent form of development that people can enjoy and feel ownership of.
- **Movement** - street have been designed to encourage way finding. Key buildings will help create a sense of direction.
- **Nature** - the existing green infrastructure has been retained and enhanced to celebrate the features of the site.



- **Public spaces** - open green space will be overlooked where possible to encourage natural surveillance. Key frontages are focused around green spaces and main movement routes.
- **Uses** - The development will deliver a positive mix of uses. Providing a shop, residential and open space.
- **Homes and buildings** - The development will aim to meet the needs of a wide range of users and meet adequate size standard on all new buildings and homes.
- **Resources** - The design will be durable and adaptable, so that it works well over time and reduces long-term resource needs.
- **Lifespan** - management and maintenance will be considered at detail stage in order to ensure the potential impacts of service charges on the future residents.

# Access & Movement

## Vehicular Movement

The site runs parallel to Sodbury Road (B4060), which runs on a north - south axis. This provides connections directly into the Wickwar village centre and Charfield to the north. To the south Sodbury Road connects Chipping Sodbury and Yate.

The B4060 connects onwards to Bristol via the A432, Gloucester via Junction 14 northbound on the M5 and Swindon using Junction 18 eastbound on the M4. These motorways are good connections to Birmingham and London.

## Public Transport

There is a bus stop located on Sodbury Road found on the northeast corner of the site. Services 84 and 85 connect to Yate, Wooton-under-Edge and Chipping Sodbury.

The closest railway station is located in Yate and can be accessed by the 85 bus service. The station has services to Bristol, Gloucester and Worcester. Changing trains services at Bristol Parkway, a 9 minute journey, offers many mainline services including Swindon, London and Birmingham.

## Pedestrian and Cycle Access

There are no existing Public Rights of Way (PRoWs) that cross the site, although there are PRoWs nearby. There is a PRoW located a short distance to the north of the site that connects to the wider network of footpaths. Currently this routes through South Farm farmstead which is not always an obvious route or inviting, there is an opportunity that an alternative route could be provided through the site and to connect to Sodbury Road to create an inviting, clear and comfortable walking route to take.

National Cycle Route 410 lies to the north of the Wickwar and forms part of the Avon Cycleway. This connects into Bristol through the surrounding town and villages.

*There is opportunity to deliver good connections to the existing pedestrian network and provide direct access to the B4060 Sodbury Road, which has public modes of transport allowing for immediate connections to the local and wider area.*

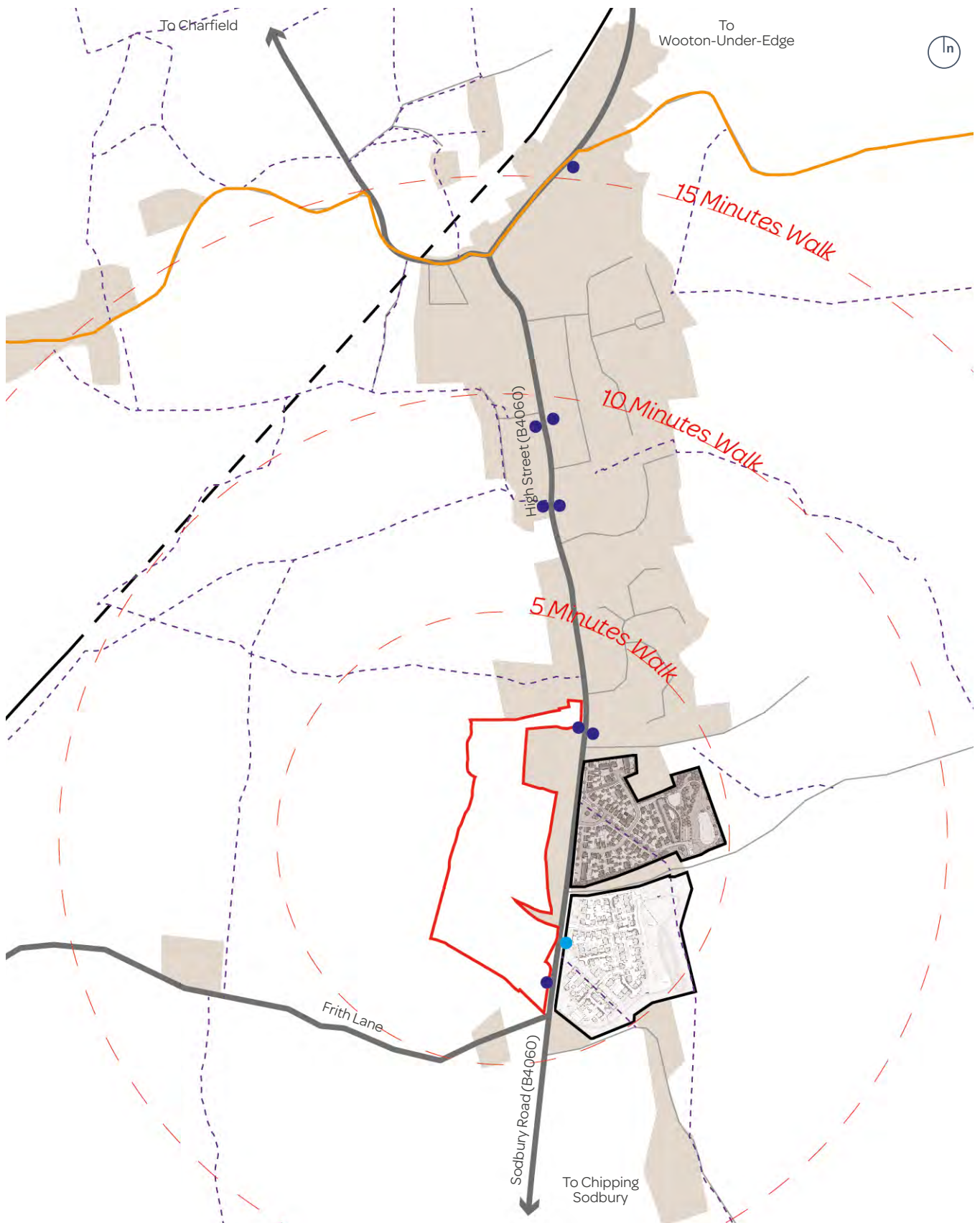
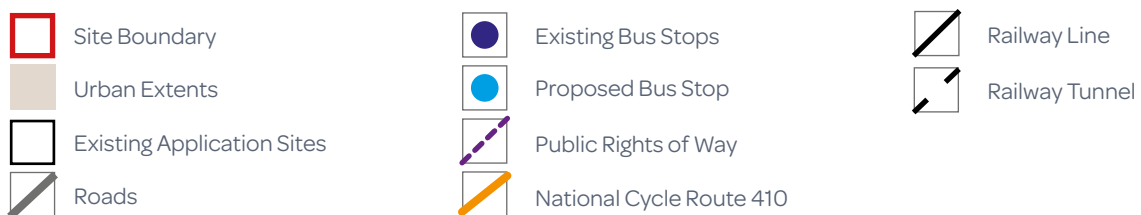


Figure 3: Movement Plan



## Facilities & Services

The Site has good access to a number of local facilities within 10-15 minutes walking distance.

The nearest facilities are found on Wickwar High Street, including The Buthay Inn, Wickwar Coffee Shop, and Wickwar Social Club. At present there is no convenience store within the village.

Alexander Horsea Primary School is located along the eastern edge of the village, and is approximately a 15 minute walk from the site.

Wickwar Playing Fields lies around a 10 minute walk to the north and is a space for dog walking, sports and informal play.

The site is extremely well positioned to a wide range of facilities and amenities in the nearby towns and villages of Yate, Chipping Sodbury, Cromhall and Charfield. These locations are easily accessible, all being within either a 10 minute drive, 20 minute cycle of the site, or by existing bus services.

Yate has a wide range of shops, services and recreational facilities, including Yate Shopping Centre and Yate Leisure Centre, along with extensive local shopping, and a range of supermarkets.

Yate comprises a choice of sports and leisure facilities including golf courses, libraries, a multi-screen cinema, and sports centres as well as a network of walking and cycle paths, parks, common and woodlands. The town has a good range of educational and health care facilities too.

Cromhall and Charfield are nearby villages to the north, with traditional high streets offering a range of local shops and food and drink local business.

Nearby industrial estates such as Wickwar Trading Estates, Beeches Industrial Estate and other employment areas such as Wickwar Concrete, Asphalt Plant and Cromhall Quarry are within an easy commute of the site and offer varied employment opportunities.

*There is opportunity for the site to accommodate a new convenience store that the village currently doesn't have.*



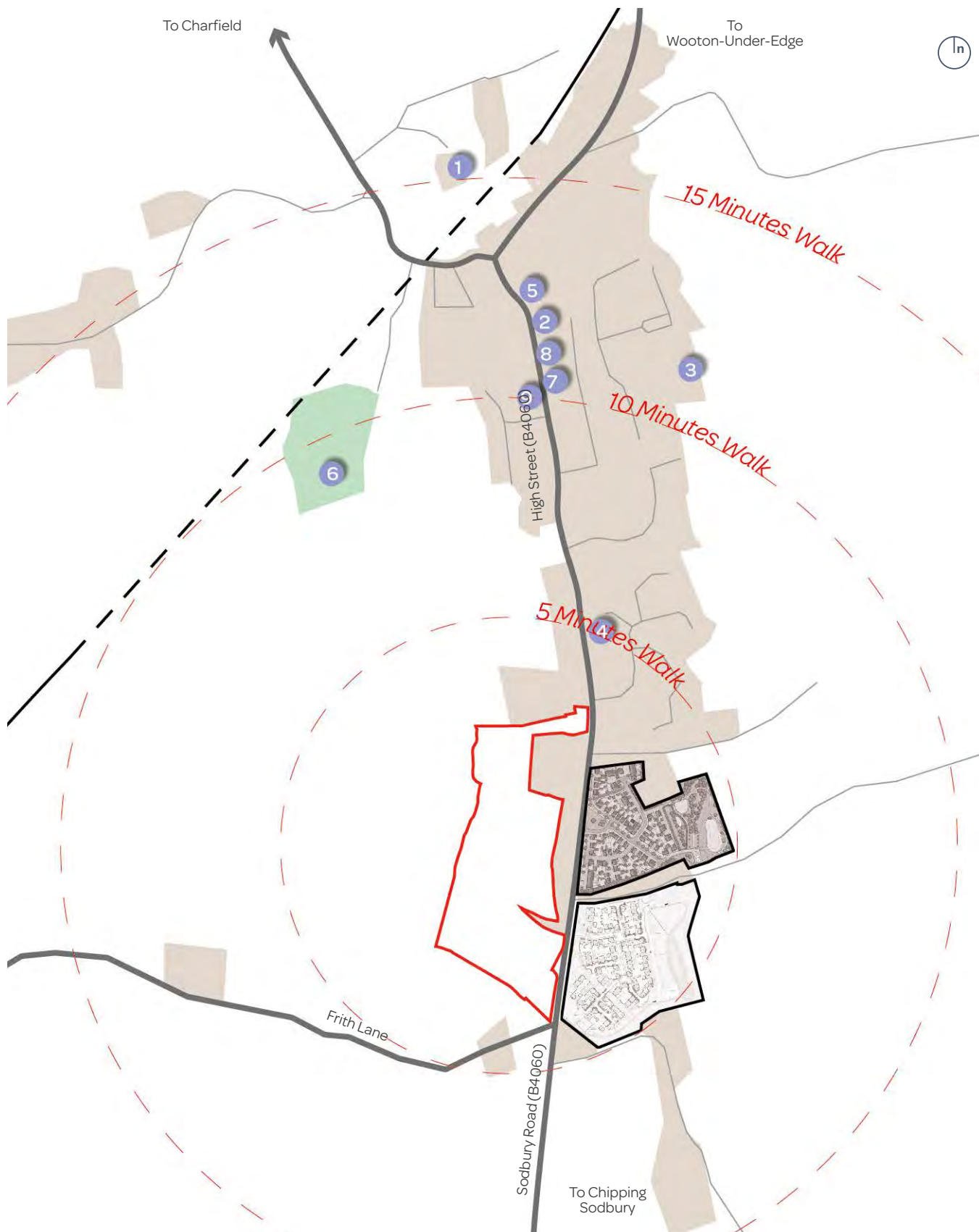


Figure 4: Facilities Plan

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|--|--|
| <span style="border: 2px solid red; display: inline-block; width: 20px; height: 10px; margin-right: 5px;"></span> Site Boundary                | 4. Wickwar Residential & Nursing Care Home |
| <span style="background-color: #d2b48c; display: inline-block; width: 20px; height: 10px; margin-right: 5px;"></span> Urban Extents            | 5. The Buthay Inn                          |
| <span style="border: 2px solid black; display: inline-block; width: 20px; height: 10px; margin-right: 5px;"></span> Existing Application Sites | 6. Wickwar Playing Fields                  |
| 1. Holy Trinity Church   | 7. Wickwar Congregational Church           |
| 2. Wickwar Coffee Shop   | 8. Wickwar Social Club                     |
| 3. Alexander Hosea Primary School  | 9. Wickwar Town Hall                       |

# Historic Development & Morphology

Wickwar has developed centrally in a linear pattern along the High Street (B4060). The settlement has seen growth to the north in the early 20th century. During the mid to late 20th century, development occurred to the east and west.

The village has since expanded in southern direction, along the B4060. Two recent applications, further this growth to the south, and were approved in 2016 and 2017, which are now under construction.

The village has an historic architectural presence in the centre with surrounding development generally comprising of 20th century detached and semi-detached homes and cul-de-sacs. Much of the development to the south in Wickwar is as a result of 21st Century development, with sinuous streets, detached and semi-detached homes and some green space.

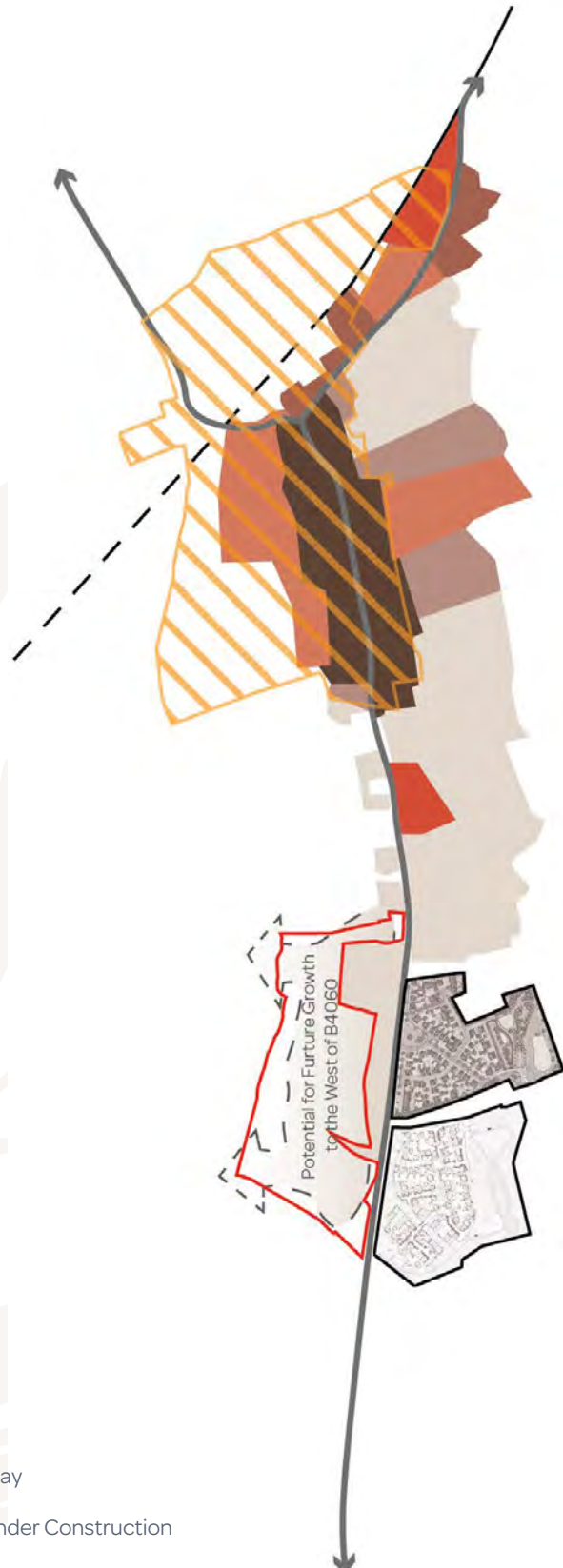


Figure 1: Historic Morphology Plan

# Context Character Appraisal

Wickwar contains a distinct character and layout which is reflective of the medieval settlements in the area. Wickwar is designated Conservation Area, in which there are a number of listed buildings.

This assessment provides an overview of the modern and historic built character that shapes Wickwar. The appraisal comments on the existing character, by analysing the architectural detail, urban and built form, as well as the streets and public realm. This will be used to inform a sympathetic, respectful and responsive design appropriate to its context in the village.

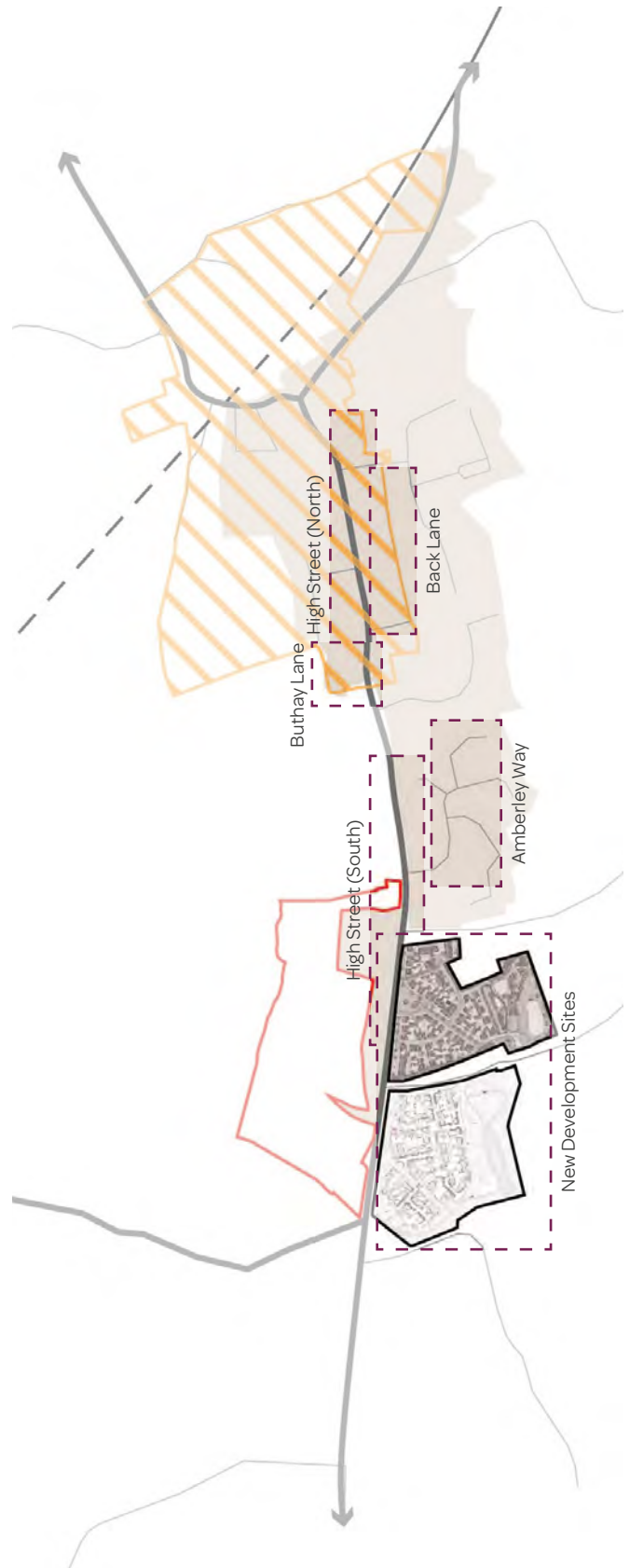


Figure 2: Context Character Appraisal Assessment Areas



## Local Context - High Street (North)

### Urban Form

- Street width is generally quite wide with close grain development.
- Plots face directly to the back of the pavement, with no set back or formal boundary treatment, such as a front garden and low height wall. (1).

### Built Form

- Plots are predominantly terraced units creating a continuous building line.
- Buildings are generally two storey in height with some examples of 2.5 storey. There are a mix of cottages and large homes and other historic buildings such as the town hall.
- Parking is predominantly on street (2).

### Roofs

- Roofs are predominantly gable type, with eaves on the street, comprising of varied pitches and some gable dormers. This created a varied roof line composition (3), including a number of chimneys (4)
- Roofs are generally grey or terracotta in colour (5).

### Façades

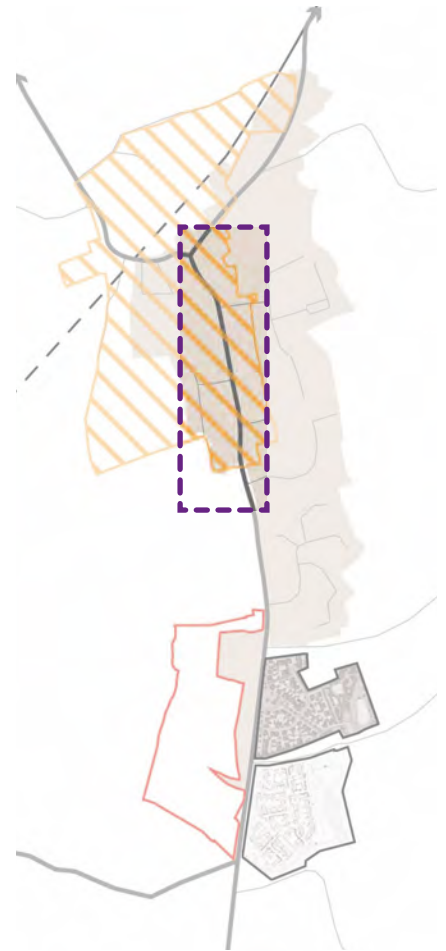
- Façades are generally quite simple, with asymmetrical features.
- Materials are predominantly limestone with some pennant sandstone, pennant stone with brick detailing or lime render to mimic ashlar masonry.





## Windows, Doors and Details

- There are some examples of bay windows (6).
- Windows are predominantly sash or sash style and are a mix of portrait and landscape in proportion (7).
- There are some examples of timber or brick lintels above windows and doors.
- There are examples of yard gateways within the street scene.



## Local Context - High Street (South)

### Urban Form

- The southern section of High Street consists of a wide carriageway and intermittent footways.
- Plots often have large detached and set back buildings with front gardens.
- One side of the road partially consists of incidental green space, which adjoins a contemporary development.
- Stone walls distinguish between public and private space (8).
- There are some examples of hedgerow planting marking formal boundaries (9).
- Where rear gardens face the pavement, stone walls are relatively high.



### Built Form

- Buildings are predominantly larger detached units.
- Buildings are generally arranged over two floors.
- Parking is predominantly on plot, such as on a private driveway or garage.

### Roofs

- Roofs are prominently gables with a uniform pitch and are grey in colour and have chimneys (10).

### Façades

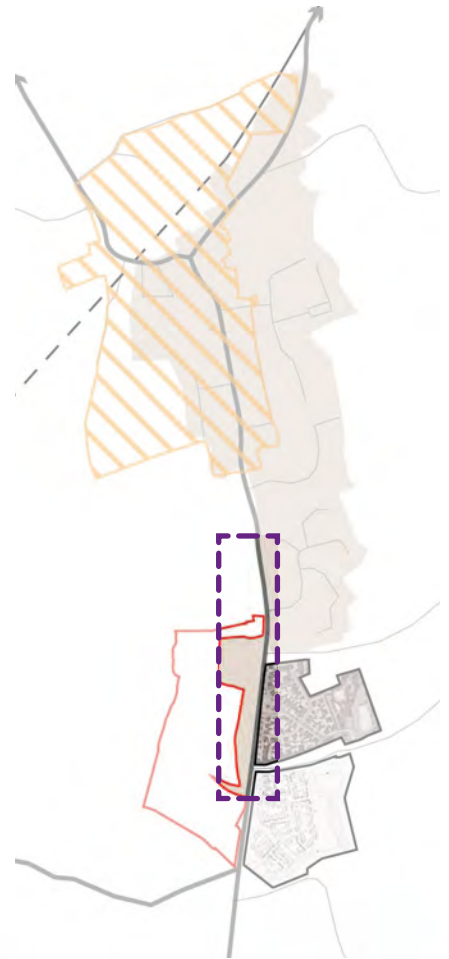
- Façades are primarily pennant stone
- There are some uses of contemporary render too. These are a mix of pastel based colours.





## Windows, Doors and Details

- Windows are a mix of sash and casement windows and are both landscape and portrait in proportion (11).
- Gable fronted stone porches, some of these are open (12).
- Wooden lintels feature above windows.
- Decorative quoining supplements rendered homes. At times these are brick or stone.



## Local Context - Buthay Lane & Back Lane

### Urban Form

- Buthay Lane and Back Lane consist of a narrow linear shared surface streets which serve historically as rear service access for the dwellings and shops on High Street (13).
- Back Lane also serves as a rear access to 20th century residential properties Inglesstone Road.
- Plots open directly onto the lane or into courtyard spaces (14). There are no front gardens or privacy margins between the buildings and the lane.
- The majority of structures on Buthay Lane are outbuildings and garaging (18).

### Built Form

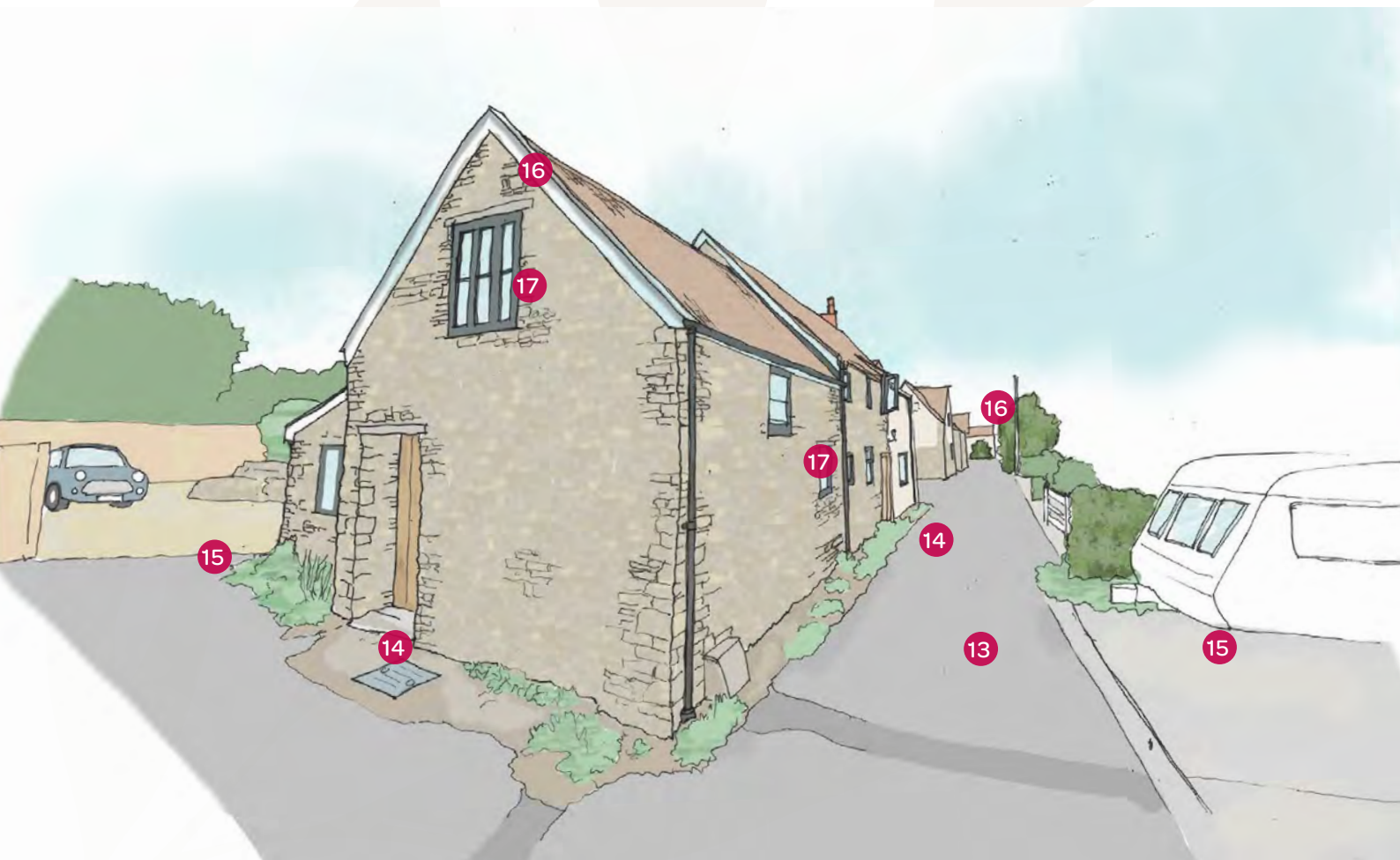
- Buildings are 1.5 or 2 storeys in height with a form more typical of outbuildings, cottages or barns.
- Between the buildings are generally high stone walls or points of access into adjacent plots.
- Parking is provided within garages, on driveways or within courtyards (15).

### Roofs

- On Back Lane roofs are predominantly gable running parallel to the street with some gable ends facing the street (16).
- On Buthay Lane, roofs are predominantly gabled with the end facing the street. A couple of buildings feature parallel gables.
- Roof materials are predominantly clay tile with slate featured on a converted chapel (19).
- Chimneys are infrequent, buildings which have historically been dwellings typically feature them, whereas gentrified outbuildings tend to omit them.

### Façades

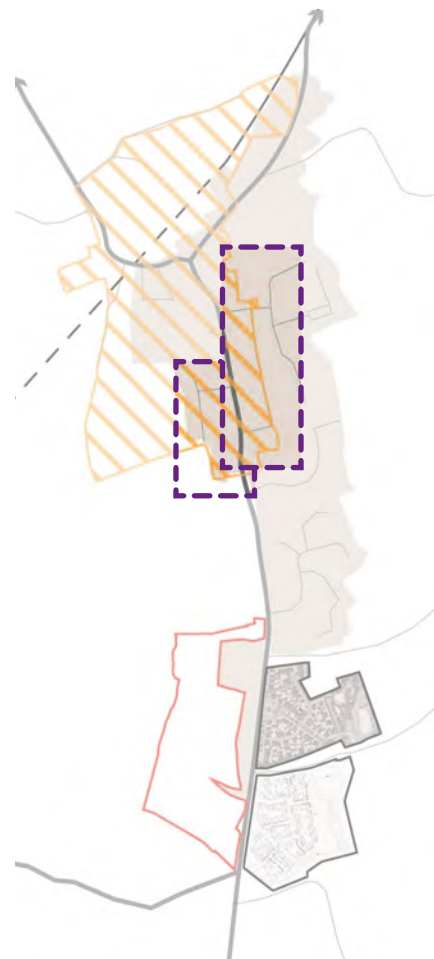
- Façades are informal and asymmetrical.
- Materials are predominantly natural coursed rubble stone with timber boarding with brick detailing in places.
- Render is also used particularly for the upper storey.





## Windows, Doors and Details

- Window openings are generally small and square in shape and feature casement openings. A larger window by feature within the apex of a gable (17).
- There are some examples of timber or brick lintels above windows and doors.
- There are examples of yard and driveway gateways within the street scene (15).
- Most of the garages and outbuildings do not feature window openings. Some however feature an apex window within the gable end.
- The old chapel features tall double-height windows (20).
- The majority of the garages feature modern steel 'up-and-over' doors.



## Local Context - Amberley Way

### Urban Form

- Sinuous residential streets with modest carriageways and footways.
- Plots have front gardens with varied boundary treatments which face the street. These include planting, low rise red brick and medium rise yellow brick walls. Some properties have no formal boundary treatment (21).

### Built Form

- Homes are generally detached and arranged over two floors.
- Parking is generally on plot, with detached garages (22).
- Homes do not necessarily front the street and are at times set back or side aligned creating a fragmented street frontage

### Roofs

- Roof are predominantly gable with the ridge line arranged parallel to the front.. Chimneys are infrequent (23).
- Roof colour includes slate grey effect or terracotta (24).

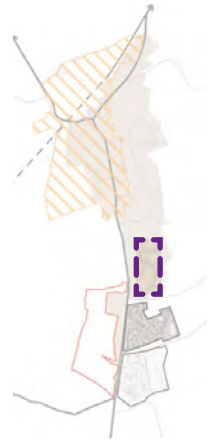
### Façades

- Frontages have a strong order with a mix of symmetric and asymmetric expression. Generally homes comprise two sets of aligned windows asymmetric or symmetrical with three sets of aligned windows , as found with the larger houses.

- Materials include stock yellow brick, red brick, and pastel render. A small number of homes are pennant stone with stock red brick quoining (25).

### Windows, Doors and Details

- Windows are a mix of casement and sash. These are generally uPVC and are white or brown in colour (26).
- Quoining features with brick dressing around windows (27)
- Open porches with small gable roofs. (28).





## Recent Developments- by Bellway Homes & Linden Homes

### Urban Form

- A mix of residential streets with moderate width carriageways, footways and shared driveways.
- Plots have small front gardens which front onto the pavement (29).
- Public facing boundaries have high brick walls.
- Hedges and estate railing are used to define the boundary from Sodbury Road.

### Built Form

- Homes are two storey, detached or semi detached .
- There are some examples of cross-gable properties
- Parking is generally on plot to the side (30).

### Roofs

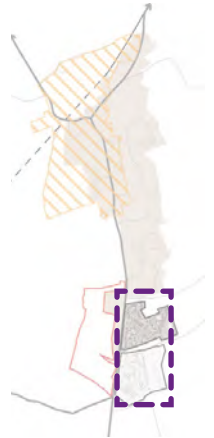
- Roofs are predominantly gable with ridge lines parallel to the front. There are homes with subservient cross gables forming projecting bays (31).
- Roof colours include slate grey and terracotta (32).
- Chimneys feature on some homes (33)

### Façades

- Façades are a mix of stone and rendered properties at the Bellway site and stone and red brick at the Linden site.
- Frontages have a strong order and are mix of symmetrical and asymmetrical. The later is generally associated with larger homes, with occasional bay windows.

### Windows, Doors and Details

- Casement windows generally in a square or landscape proportion. (34).
- Windows are dressed with rendered decorative lintels and cills.
- Windows are uPVC and are white in colour
- Front door canopies are either a projecting pitch or flat roof (35).
- Dormers feature on some homes.



## Character Appraisal Summary

### Urban Form

- High Street features a largely formal character with a mixture of symmetrical and asymmetrical buildings with a consistent and continuous build-line.
- Back Lane and Buthay Lane feature a more informal character of asymmetrical buildings and a mixture of outbuildings, converted barns and boundary walls.
- The converted Chapel on Buthay Lane is unique in being a formal building on a street composed of largely informal outbuildings and boundary walls.
- Recent growth to the village comprises modest and detached buildings.

### Built Form

- Dwellings within Wickwar are between 1.5 and 3 storeys. Taller dwellings feature primarily along the main High Street whereas Back Lane and Buthay Lane feature buildings more typical in scale to outbuildings or barns.
- Parking is provided in a variety of methods depending on the character area; High Street is wider than most and features perpendicular and parallel parking bays with minimal on-plot parking. Garaging for dwellings and businesses along High Street is accessed via Back Lane, Buthay Lane and other back of plot service lanes.

### Roofs

- Eaves run parallel to the street and comprise of varied pitches, with some gable dormers facing the street. The roofline is varied within all of the historic areas.
- The predominant roofing material within the historic areas of Wickwar is Roman style clay tile. Slate is used on a handful of buildings on High Street.
- The recently developments at Pennington Road and Tanner Way feature a mixture of terracotta and grey plain tiles.

### Façades

- Walling materials within historic areas are predominantly coursed rubble limestone and painted render.
- New dwellings at Pennington Road and Tanner Way feature a mixture of natural stone, reconstituted stone and painted render in prominent areas. Red and bluff coloured brick has been used in areas which don't face the main road, Sodbury Road B4060.

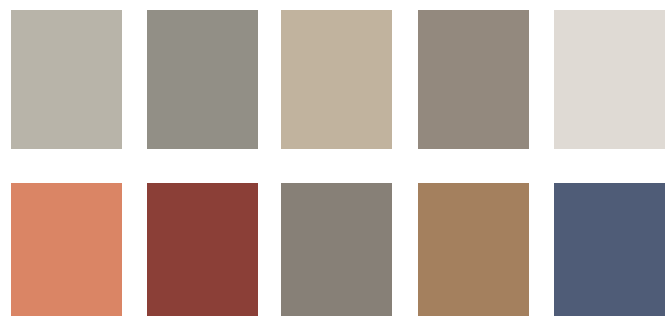
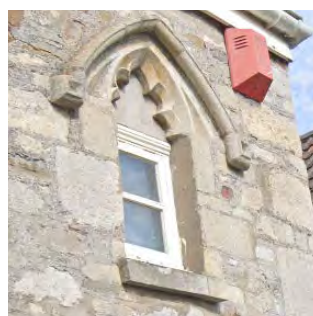
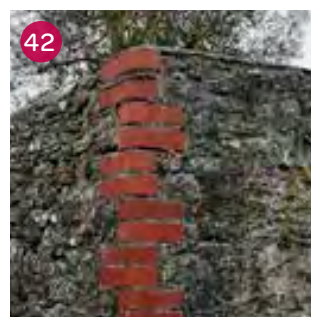
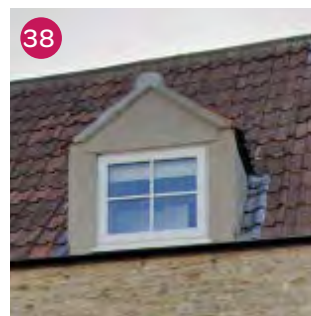
### Windows, Doors and Details

- Some buildings and structures along the High Street feature ashlar stone quoins and stone detailing. Brick detailing becomes a noticeable feature on Back Lane and Buthay Lane.
- Timber lintels are a recurring feature of many older buildings on High Street and Back Lane. Other buildings can feature decorative or rendered lintels and cills.
- High Street's buildings have predominantly white painted timber sash windows with some casement windows. Structures on Back Lane feature smaller casement windows.
- Gabled dormers feature on older 2.5 storey buildings. Where buildings have been converted to utilise their loft space rooflights are used.
- The converted chapel on Buthay Lane uniquely features 2 storey double height windows.
- Adapted buildings and twentieth century homes generally comprise casement windows that are uPVC and white in colour.



## Distinctive Features

- Slate is used sparingly on a handful of buildings on High Street (36). Clay tile is the dominant material in the historic areas of Wickwar.
- Details such as lintel courses have in places been constructed from brick. This composition of primarily natural stone walling and brick detailing can be found throughout the village (37, 40, 42).
- Where dormers are used, they are gabled (38).
- Timber lintels feature on many older buildings on High Street (39).
- On informal structures and outbuildings timber weatherboarding is sometimes used on the upper storey (41, 43).



## Landscape & Visual Impact

A landscape and visual baseline assessment has been undertaken by EDP, in support of the development of land at Wickwar and has been submitted as part of this application.

There are no national or local landscape designations within or adjacent to the site.

The site and its surroundings fall within the north-eastern extent of NCA 118 'Bristol, Avon Valleys and Ridges' National Character Area, which encompasses the City of Bristol at its core and the surrounding area to the north-east, south and south west - including the Chew and Yeo valleys, Keynsham, Clevedon, Portishead and parts of the Cotswolds and Mendip Hills Areas of Outstanding Natural Beauty.

The site is situated wholly within Landscape Character Area (LCA) 5 - 'Wickwar Ridge and Vale', described briefly as "a diverse undulating landscape covered with a mix of farmland, woodland and common."

In terms of wider topography, the landscape to the east of the site and Sodbury Road slopes gently downward away from the site, seemingly positioning the settlement of Wickwar upon a slightly elevated ridgeline, as identified within LCA 5 of the South Gloucestershire Landscape Character Assessment SPG.

This character of the site is able to accommodate change without fundamentally changing characteristics of the landscape character area, as such, is considered to be of low sensitivity.

The generally flat topography of the site and surrounding landscape gives the site an open character. The site has some long-distance views to the west, with movement of traffic upon the M5 motorway visible in the distance.

From within the site, there are views towards the tower of Holy Trinity Church, to the north of Wickwar. Views to the east are limited by a combination of existing built form and the downward sloping topography. There are clear views from the surrounding homes into the site.

The conservation area is visible to the north of the site. These views however do not interfere with the views towards Holy Trinity Church and as such is deemed to be of low sensitivity in terms of historic landscape character and development will not have a detrimental effect if designed sensitively.

*Development creates an opportunity for a softened transition in built form from urban to rural and orientate buildings to frame views to the open countryside. Green infrastructure should be retained and enhanced particularly along the western site boundary helping to frame long distance views to the west and respond positively to the rural character of the area.*





Figure 3: View looking north west from Sodbury Road



Figure 4: View looking north east from Frith Lane



Figure 5: View looking east from Public Right of Way (LYA/11)



Figure 6: View looking south east from Public Right of Way (LWR/23)



# Heritage and Archaeology

An archaeological and Heritage Baseline Assessment has been undertaken by EDP in support of the development has been submitted as part of this application.

The assessment concludes that the site does not contain any designated heritage assets or form part of a wider designation such as a World Heritage Site, registered battlefield or Conservation Area, that would otherwise effect its development.

The site does however, form part of the of the setting of designated heritage assets, including the Wickwar Conservation Area (350m north of the site) and the Grade II listed farm house 40m to the north of the site.

The assessment undertaken demonstrates that development within the site would cause no more than ‘less than substantial harm’ to the heritage asset or designation.

The Historic Environment Records (HER) derived from the National Mapping Programme (NMP) relate to two areas of ridge and furrow cultivation earthworks that were recorded from historic aerial photographs across two of the fields within the site. Following a site visit undertaken by EDP, neither field was seen to contain ridge and furrow earthworks and it is presumed that the earthworks visible in the assessed aerial images have since been ploughed away by modern farming.

There are existing views in the local context that will need to be considered in the masterplanning proposals.

*The proposed development has the opportunity to provide appropriate screening and buffering through additional tree planting, to help retain Wickwar’s rural character and respond positively to existing heritage considerations.*



Figure 7: View south towards the site from the open area of Wickwar Conservation Area showing view through centre of the site



Figure 8: Grade II Listed South Farmhouse, north of the site



Figure 9: View south west towards the site from the edge of the conservation area illustrating rural setting

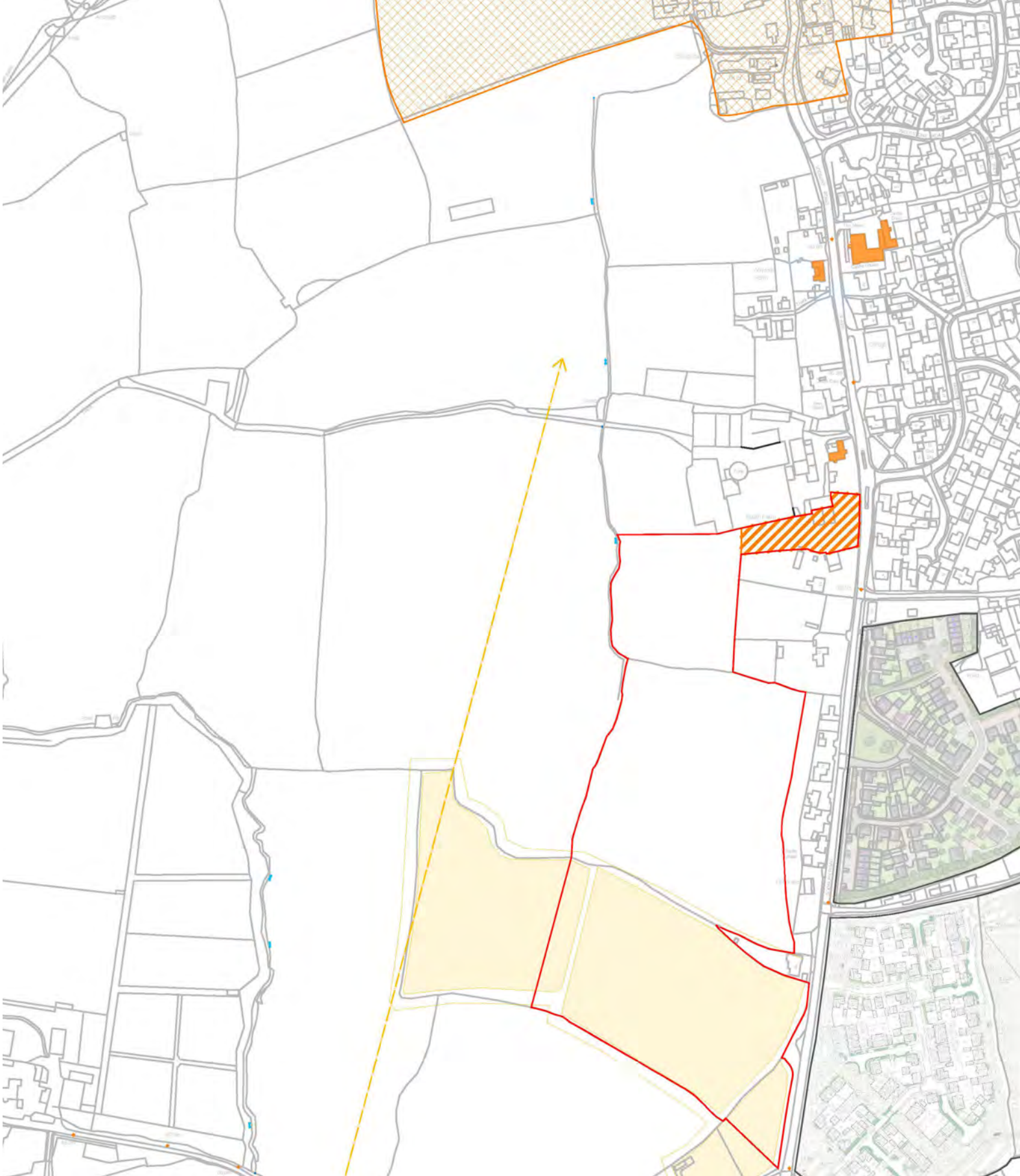


Figure 10: Heritage and Archaeology Plan

- |   |                           |   |  |
|---|---------------------------|---|--|
|  | Site Boundary             |  | Area of site that is closely related to the setting of the listed farm house |
|  | Wickwar Conservation Area |  | Presumed lost earthworks due to agricultural activity                        |
|  | Grade II Listed Buildings |  | Views towards Holy Trinity Church tower                                      |

## Topography

The site topography is generally flat. It ranges from approximately 90m AOD at its highest point in the south east corner to 83m AOD at its lowest point to the north west. The sites existing topographical context creates a central shallow depression within the site.

*The masterplan can work within the sites existing topographical context to create a fully accessible scheme for all users..*

## Flooding

The site lies within Flood Zone 1, meaning it isn't subject to flooding as a result of rivers or the sea. However the sites topography and central running depression results in surface water flood risk.

Future development will reduce the permeable area and need to manage surface water run off using sustainable urban drainage.

There is an existing culvert headwall that lies within the site that drains existing runoff from the B4060. Further surveys are required to determine the exact location of the direction of discharge flow across the site so this can be integrated into the drainage strategy appropriately..

*The masterplan will propose an appropriate flood mitigation strategy in the form of Sustainable Urban Drainage Strategies (SUDS) and will include an attenuation pond and Swale. These will accommodate the existing surface water flood risk, flood risk as a result from the proposed development and an allowance for Climate Change.*



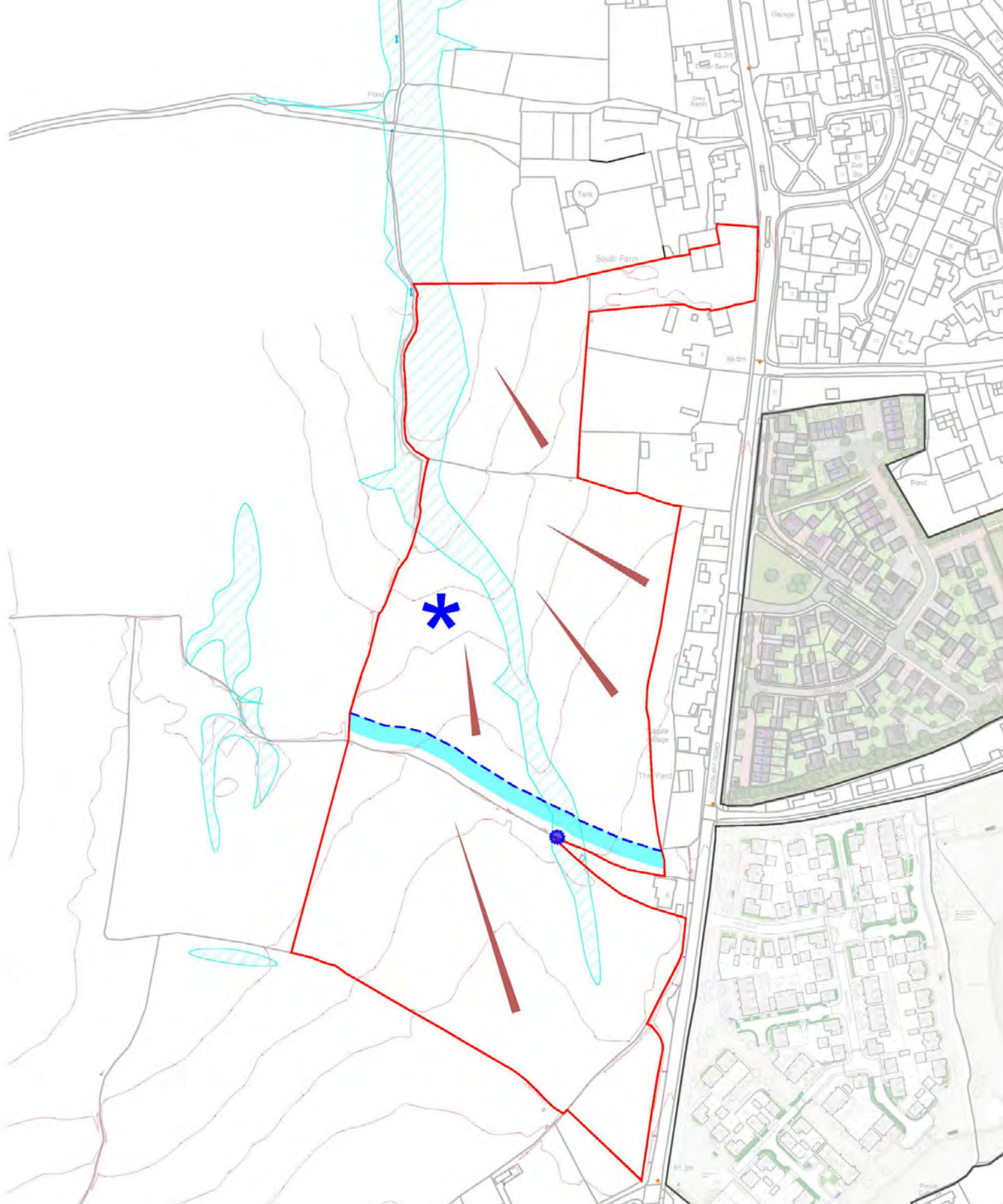


Figure 11: Flooding & Topography Plan

- |   |                                       |   |  |
|---|---------------------------------------|---|--|
|  | Site Boundary                         |  | Indicative Swale Location including 10m buffer |
|  | 1m Contours                           |  | Existing Culvert Headwall Location             |
|  | Extent of Surface Water Flood Risk    |  | Direction of Slope                             |
|  | Indicative Flood Attenuation Location |   |  |

## Trees and Hedges

An Arboricultural Impact Assessment has been undertaken by RSK to which the findings are set out in detail within the associated document.

A total of 23 individual trees, 4 tree groups and 13 hedges have been recorded within the survey. Of these, it was concluded there were;

- 1 Category A Trees
- 9 Category B Trees & 2 Category B Tree Groups
- 10 Category C Trees & 2 Category C tree groups
- 2 Category U Trees
- Existing Hedgerow to the site extents and internal field boundaries (Please see ecology plan for information in regards to value of hedgerow).

As part of the proposals existing Category U trees will be removed, due to impaired condition.

All other trees and/or hedgerow will be retained where possible. Associated Root Protection Areas (RPA's) have been protected for each Category A, B and C tree and hedgerow and development should not occur within these areas in order to preserve and protect the tree.

Where trees may be removed, these will be appropriately mitigated, through new landscape planting as part of the proposals.

*The masterplan will seek to retain and enhance, where possible, existing trees and hedgerow. Additional planting will be provided within the scheme, through street tree planting and within proposed green spaces.*



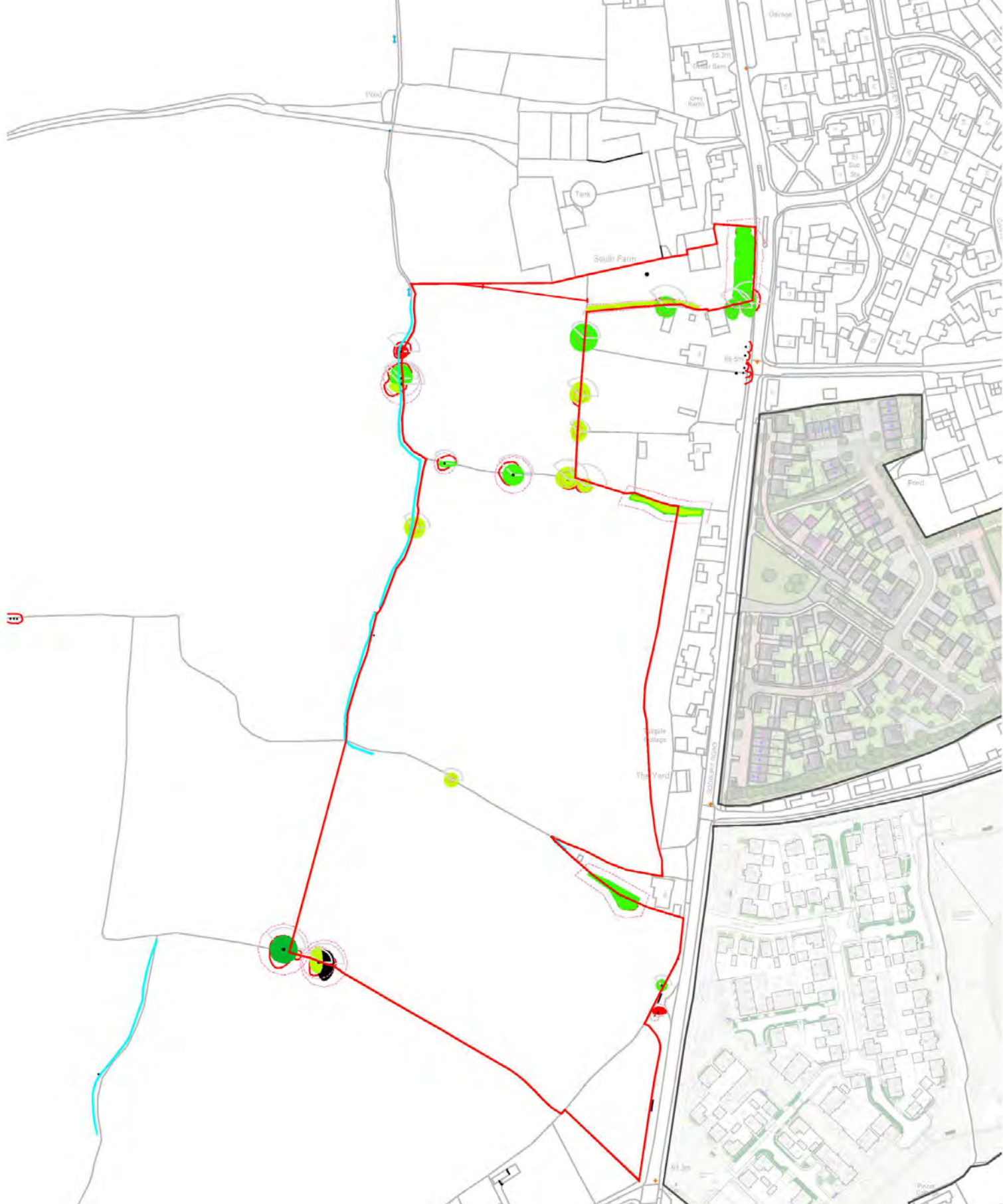
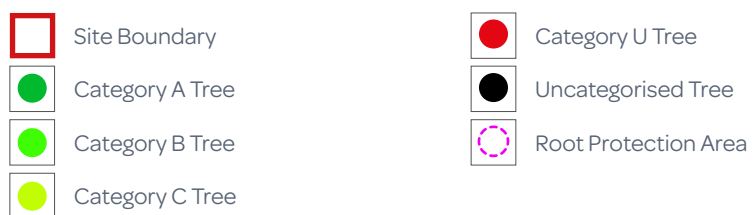


Figure 12: Flooding & Topography Plan



## Ecology

An extended phase 1 habitat survey has been undertaken by EDP in support of this application.

It concluded that the existing habitats of arable land and agriculturally improved grassland predominant within the site is considered to be of low ecological value. However, the existing native hedgerow network and mature trees are considered to be of good ecological value with potential to support protected and notable species.

### Bats

The survey identified one tree with high bat roost potential and four trees with moderate bat roost potential.

The site's existing hedgerow contain suitable foraging habitats for bats too. These features will be retained, where possible as part of the masterplanning proposals, and appropriate mitigation strategies will be implicated, where necessary, to prevent any adverse impact

### Dormice

The site contains a relatively extensive hedgerow network with good connectivity to additional hedgerow habitat present across the wider landscape.

No evidence of dormice were recorded during any of the survey visits undertaken across 2020 and 2021.

A small number of wood mice and evidence of wood mice, namely wood mouse nests, were recorded across the Site during the undertaken surveys.

### Great Crested Newts

No water bodies are identified within the application site. However a single water body is located approximately 10m north of the site. The Habitat Suitability Index (HSI) assessment concluded that the water body was to be of good suitability to support Great Crested Newts (GCN) and was confirmed with eDNA testing.

During a site visit no GCN were identified and as such it is considered to contain a low population.

An additional pond with confirmed eDNA testing is located greater than 500m from the site, and as such, potential impacts on this population are unlikely.

### Breeding Birds

The overall abundance of birds recorded is not regarded as being important or exceptional for any species.

Breeding bird species recorded on-site is considered to be typical for the range and quality of habitats present, and for its location. From the survey data, a greater number and diversity of birds is present around the hedgerows within and adjacent to the Application Site.

Existing hedgerow habitats offer greater opportunities for nesting and foraging birds and for a wider range of species in comparison to the open pasture which dominates the Application Site. These habitats should be sought to be retained and enhanced where possible as part of the development proposals.

### Badgers

No active setts were identified within the Application Site, although four setts were identified within the wider survey area including three active subsidiary setts. Hedgerow boundaries provide suitable cover for badger setts whilst arable land and improved grassland within and adjacent to the Site provides suitable foraging habitat.

*The masterplan will seek to mitigate and respond positively to the existing ecological considerations by proposing appropriate buffers from relevant trees and/or hedgerow, as well as providing additional ecological enhancements to help encourage wildlife to use the site*

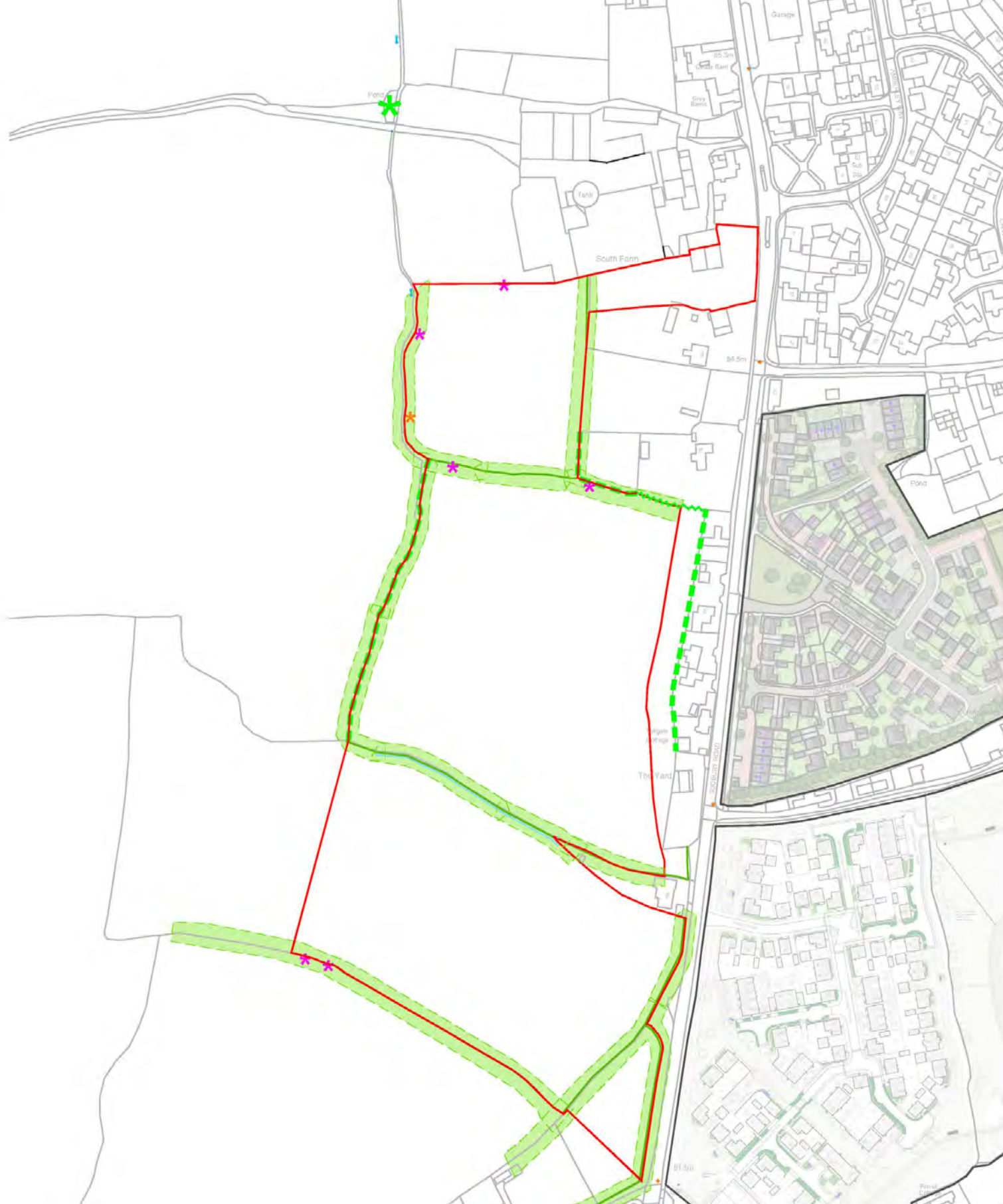









Figure 13: Ecology Plan

- |   |  |   |   |
|---|--|---|---|
|  | Site Boundary                                |  | Intact Species Rich Hedgerow            |
|  | Trees with High Bat Roost Potential          |  | Intact Species Rich Hedgerow with Trees |
|  | Trees with Moderate Bat Roost Potential      |  | Intact Species-Poor Hedgerow            |
|  | eDNA confirmed pond for GCN (low population) |  | Hedgerow Buffer from existing hedgerow  |



## Access

A transport and access strategy assessment has been undertaken by Clarkebond and has been submitted as part of this application.

The site abuts the B4060 Sodbury Road to the east. The transport assessment proposes two access points into the site to serve the site.

An access point will be located to the north of Poplar Lane junction and south of the Amberley Way mini-roundabout. Another primary access point is proposed to be taken south of the Horwood Lane junction. Both of proposed access points will be priority T-junctions.

The primary street has the potential to support public transport access.

*Key walking and cycling routes for residents on the proposed development will be primarily to the north where the local facilities are located, and thus the northern access will be key for pedestrian and cycle movement.*

## Utilities and Services

A Utility search has been undertaken by Premier Energy as part of the application process.

There are a number of utilities within the immediate context of the site and are mostly found on the B4060 Sodbury Road.

A Low Voltage overhead transmission line crosses the north of the site from east to west. These will be grounded as part of the proposals.

A high pressure gas pipeline is situated to the south west of the site and runs on a north west - south east axis. The Health and Safety Executive (HSE) has outlined Outer zone that enters the site to the south west.

## Noise

A noise assessment has been undertaken by Wardell Armstrong LLP to accompany the application for Land at Wickwar.

Noise sources that have the potential to adversely affect the proposed development are from the proposed traffic from the B4060 Sodbury Road and industrial noise associated with the dairy farm to the north and the Country Style supplies to the east of the site.





# Summary of Site Considerations

The masterplan should:

- Accommodate a new shop to help serve the proposed development as well as the existing residents of Wickwar and help to boost the local economy.
- Use appropriate massing, scale and landscape design to respond to the landscape character context, and create a transition from the urban edge of Wickwar to the rural landscape to the west.
- Respond positively to the associated heritage assets through appropriate landscape design, built form and scale in the north of the site.
- Utilise the site's existing topography to ensure the site sits within its context and the layout and street gradients are accessible to all.
- Incorporate a drainage strategy that uses the site's topography and mitigates against any increased flood risk as a result of the proposed development, as well as an allowance of 40% for climate change.
- Retain and enhance, where possible, the site's existing vegetation and provide additional planting to help reinforce the sites existing green infrastructure network.
- Deliver an appropriate ecological strategy that will seek to retain and enhance, where possible, existing habitats. Where habitats are lost these will be compensated elsewhere on the site. Species rich planting will be provided to increase biodiversity and encourage wildlife to use the site.
- Create two new access points into the site from the B4060 Sodbury Road, with the opportunity to reroute the bus service through the site.
- Utilise existing pedestrian links within the wider context of the site and create a new integrated pedestrian network.
- Appropriate buffers will be provided within the masterplan to existing noise risk and utility equipment.

	Site Boundary		Intact Species Rich Hedgerow with Trees
	Other Planning Applications		Intact Species-Poor Hedgerow
	Proposed Vehicular Access		Hedgerow Buffer from existing hedgerow
	Key Views		eDNA confirmed pond for GCN (low population)
	1m Contours		High Pressure Gas Main
	Extent of Surface Water Flood Risk		Inner Zone
	Indicative Flood Attenuation Location		Middle Zone
	Indicative Swale Location including 10m buffer		Outer Zone
	Existing Culvert Headwall Location		Overhead LV Transmission Line
	Hedgerow		Underground LV Transmission Line
	Category A Tree		Potential Access Point
	Category B Tree		Public Right of Way
	Category C Tree		Grade II Listed Farmhouse
	Category U Tree		Setting of the Grade II listed farmhouse
	Uncategorised Tree		National Mapping Programme (NMP) Records
	Root Protection Area		Possible Shop Location
	Trees with High Bat Roost Potential		
	Trees with Moderate Bat Roost Potential		
	Intact Species Rich Hedgerow		







# Evolution





# Consultation

## Design Review Panel

The scheme was reviewed by the Design West South Glos. Design Review Panel in June 2022 to instigate a constructive and open engagement upon the proposal.

### Comments & Response:

**Site Drainage** - The Panel suggest the SuDS provision should not be concentrated in a single large attenuation feature, and suggested dispersing the SuDS provisions through a range of design elements including rain gardens so that it could be integrated more successfully into the public realm.

*The drainage strategy has reviewed and the attenuation basin provision has been split into three smaller attenuation basins to distribute the provision. Alongside this, a rain garden has been integrated into the verge of the primary street.*

**Landscape** - We noted that the public open space (POS) provision was concentrated along the open boundary to the west. We appreciate that surface water flow across the site had resulted in the current solution, but perhaps this is too engineering led and that an alternative solution may offer more overall value and benefit. The current layout, in placing the majority of POS to the west, means only a small proportion of the homes have aspect over significant POS. The POS appears peripheral to the scheme, whereas an alternative layout may place it as a more central element, offering greater potential for social interactions and improved aspect to homes within the body of the site (the houses along the west will retain an aspect over largely open countryside).

*The play areas have been moved from the western open space into the open space alongside the E-W hedgerows and rain gardens have been incorporated into the primary street to increase the amount of open space centrally and ensure all homes have aspect onto green infrastructure.*



Figure 16: Outline Application Masterplan - February 2022

**Transport and Connectivity** - There are currently two access points (both off Sodbury Road) around the perimeter of the site. We noted that the PRow through South Farm appeared underused. We suggest there may be benefit to the walking network, and to the farm itself, if an additional Non-Motorised User (NMU) connection was made through the northern part of the site, linking with the existing PRow to the north west. This would utilise a field which is not part of the red line, but we understand is within the same land ownership. Coupled with routes through the site, and improved crossings over Sodbury Road, there is real potential to enhance the PRow network, with health and wellbeing benefits.

*A new pedestrian link has been incorporate into the masterplan through the northern part of the site, neighbouring field and linking to the PRow to provide an alternative route to the existing underutilised PRow access running through South Farm.*

**Heritage** - It was felt that the mitigation measures proposed for any potential harm to the setting of the two farmhouses was appropriate, but that it was important that this is taken through into detailed design proposals, considering the impact of items such as signage and lighting.

*The comments were noted and welcomed.*

**Character** - With respect to the character of the area, the Panel suggests that the proposals should not seek to mimic the village centre, but rather seek to ground the proposals in their context through using similar patterns of variety in building form, material and detail. Commonly with new residential developments of this type, achieving this variety in an appropriate manner can be challenging, but we nonetheless recommend the design team seek ways to deliver this approach and to avoid potential perceptions of uniformity and banality which delivering repeated standard house types can produce.

*The comments were noted and the Character section of this DAS has been expanded to provide greater clarity on the aspirations for the scheme, which would inform future Reserved Matters applications .*

**Public realm** - We support the design team in their aim to create distinctive and appropriate public realm within and around the scheme. We also mention in the landscape section that the disposition of POS across the site could be reviewed. In this context there appears to be a potential solution that creates a greener heart to the development and in doing so would establish more opportunities for social engagement within a characterful neighbourhood place in the centre of the development. Reconsidering how play spaces for children and adults may contribute to this option for place making is something that might also be usefully considered.

*The play areas have been moved from the western open space into the open space alongside the E-W hedgerows in response to these comments.*

We noted that the alignment of some streets, with some minor revision, may also allow distant views to the Church tower from some viewpoints. If this was achievable it would help root the development into its existing village context.

*Reviewing the masterplan, we have identified that a view to the Church tower will be available along the primary street in the northern parcel of the site. A framed view to the tower will be available as the primary street runs through the gap in the hedgerow.*

## Officer Comments

### Urban Design Officer

**Masterplan** - The general arrangement of parcels within the site makes logical sense, with green buffers to the sides of the existing GI and another buffer along the western edge. The two access points would allow for a circulation through the site and could help spread the intensity of traffic on each junction at peak travel times.

There is a general lack of detailed information in terms of how the proposed number of units can be accommodated within the site. This is needed to ensure that the number of dwellings can in reality be accommodated. For example, it is customary for Outline applications to provide indicative layouts which demonstrate the deliverability of the numbers of dwellings. This process also allows the slightly more strategic road layout to be tested and ensures deliverable blocks.

*An illustrative layout has been submitted with this revised Masterplan to demonstrate the deliverability of the scheme in accordance with the masterplan.*

**Character Context** - There is a lack of thorough context and character appraisal. The character appraisal (page 18 of the DAS) focusses on the High Street but the back lanes (Buthay and Back Lanes) are equally locally distinctive and contain numerous interesting and locally distinctive features. The site visit identified clear positive character features, including hierarchy of development pattern and principal materials. Although illustrations provide some context, photos would be more helpful.

*The character assessment in this DAS has been revised to respond to these comments to include precedents from Buthay and Back Lanes and including more photographs.*

**Proposed Building Heights** - Page 47 of the DAS sets out the building heights but is very general. It essentially says that the whole development (except the retail use) could be up to 2.5 storeys. While I would not object to the use of 2.5 storey elements, these should be focussed along the primary circulation routes, and at the end of terminal vistas, with lower, mews and more standard

house types off the primary route and to the rears of the principal dwellings, which would reference the historic development patterns found locally. This approach should be linked with the street hierarchy information, provided in subsequent pages.

*Whilst a range of building heights was always the intention of the original submission, with the heights varied to respond to Character Areas, the Building Heights Plan has been revised to clarify this distribution.*

**Parking** - This needs to be linked to the movement and built form hierarchy, with the higher-density approaches along the primary route and lower-density towards the edges. Where parking will be provided on both sides of the primary route, it is sensible to indicate different approaches to parking, as this will help reduce the visual impact within the streetscene. For example, where perpendicular frontage parking is provided on one side of the street, on the opposite side, parking between buildings would reduce visual impacts.

*This is noted for the Reserved Matters and has been reflected in the updated Character Areas proposals.*

**The Shop** - The location of the shop makes sense from a visibility perspective from the main road but it is positioned at nearly the far southern point of the site area, at the point furthest from the existing built-up area. While this location would benefit the population in the immediate vicinity, it would be less accessible to the population to the north. Given the lack of safe crossings, this is a further issue.

*This location has been carefully considered to respond to input provided by potential operators who are interested in delivering the local shop. NRP have calculated that the proposed shop is within a 19 minute walk of all existing residents of the village and the revised scheme includes a revised junction design and highways measures to facilitate safe crossing.*



# Engagement

## Meeting October 2022

Following a review of all the consultation feedback, we had a meeting with the Case Officer and Urban Design Officer from South Gloucestershire Council to discuss the way forward and our proposed response to the consultation comments.

We presented our proposed response to the DRP and Urban Designer's comments including:

- Summary of DAS updates – including the revised local character assessment and updates to include NDG,
- Explanation of Masterplan amendments including revised basin design and moving play areas,
- Parameter plan changes to Building Heights Plan,
- Revised Character Areas,
- Illustration of how the allotment area can be accommodated,
- Explanation of shop location with walking distances, and
- Illustrative layout to demonstrate how 180 homes could be accommodated.

During the meeting we agreed to:

- Extension of time to allow submission and consultation on updated package of information – objective is to make one updated/supplementary submission to deal comprehensively with comments raised to date, including an updated DAS with illustrative masterplan, as well as addendum to other supporting technical documents as appropriate;
- Addition of street sections for different street typologies to be added,
- Proving layouts to be provided for typical areas of site comprising each street typology, to include parking and provision for services/lighting/street trees,
- POS typology areas to be provided to ensure that at least minimum provision is being made to ensure policy compliance, and
- Further clarity on proposed scale/form of drainage attenuation features to be considered for appearance/function as part of wider landscape.

## Design Principles



### Connections

The site abuts the B4060 Sodbury Road. There is opportunity to deliver two new access points into the site, which will serve the development.

There are existing PRowS around the site, opening the opportunity to utilise these routes and create new connections between the footpaths to form a more permeable pedestrian network within Wickwar.



### Flooding & Topography

The site is subject to surface water flood risk, and any proposed development will reduce the amount of permeable surfaces within the site. The masterplan will deliver a comprehensive flood mitigation strategy to ensure new and existing flood risk is managed sustainably and designed to promote biodiversity within the site.



## Trees & Hedges

The site has a strong Green Infrastructure (GI) network. The masterplan should seek to retain and enhanced where possible, the sites existing trees and hedgerow, and use these features to help frame key views. Where vegetation is lost, the proposals should seek to compensate through additional planting.



## Ecology

The site sites existing trees and hedges have a degree of ecological value. There is opportunity for the masterplan to retain and enhance the sites existing ecological features through green corridors, to help encourage wildlife to use the site.







# Design



# The Proposals

The development is proposed to be residential-led development comprising of new homes of various sizes and tenures, including family housing.

The development proposals respond to the context and design considerations that have been described within the assessment section of this document.

The masterplan proposal is generally consists of 'outward' looking perimeter blocks, whereby residential units face the public realm and are proposed to be back to back/back to side development. This approach promotes natural surveillance and provides overlooking onto all streets and spaces.

The design has used the existing landscape features to inform the design. The established trees and hedgerows, together with the topography of the site inform the structure of the masterplan.

Two new vehicular access points are taken from B4060 to create a primary street that connects these two points; leading through the masterplan. This primary street forms a spine within the masterplan and has a series of secondary side streets extending to all homes within the masterplan.

The masterplan creates a desirable place to live, and will integrate into the landscape, enhancing its best assets such as the hedgerows and potential habitats. Attractive new homes will sensitively respond to the character of the village with access to high quality amenity space and the countryside.

Existing trees and hedgerows are enhanced with proposed new planting that includes native species, as well as a new hedgerow corridor. This will reinforce the existing landscape context and support ecology and biodiversity.

A new allotment area is proposed to the north of the site allowing for a space for the community to grow fresh fruit and vegetable. This will help to improve health and well-being and also have a degree of educational value for younger members of the community and is accessible for new and existing residents of Wickwar.

Two play spaces have been incorporated into the heart of the masterplan to provide opportunities for social engagement as people move through the streets and open spaces and create a characterful neighbourhood heart.

-  Site Boundary
-  Residential Development
-  Area Reserved for Potential Shop
-  Open Space including informal recreational and natural and semi-natural urban green space
-  Key Spaces
-  Allotments
-  Public Rights of Way
-  Primary Streets
-  Areas of boundary to be bolstered with additional planting
-  Existing Hedgerow
-  Existing Trees
-  Proposed Pumping Station
- 1** Proposed Access Points
- 2** Proposed Local Area for Play (LAP)
- 3** Proposed Local Equipped Area for Play (LEAP)
- 4** Proposed attenuation basins
- 5** Proposed Swale
- 6** Proposed Rain Garden
- 7** Alternative pedestrian walking route to connect to the existing PRow
- 8** Framed view to Holy Trinity Church
- 9** Indicative routes of proposed pedestrian links through open space





Figure 17: Proposed Framework Masterplan



## Land Use

The masterplan can deliver up to 180 new homes within a net developable area of 4.46 ha. This equates to an approximate density of 40 dwellings per hectare.

The development includes a variety of house types, ranging from 1 bed flats to 4 bedroom homes. The development will include a range of housing tenures, including open market and affordable units.

The affordable housing will make up 35% of the development, to help meet local needs for affordable homes.

Higher density will be found along the primary street and lower densities are located on its edges, to appropriately address the context of the development's green edge.

The scheme will also deliver a new shop to help serve the proposed development as well as the existing residents of Wickwar and help to boost the local economy. This has been located to the south the site to help create a sense of approach into Wickwar.

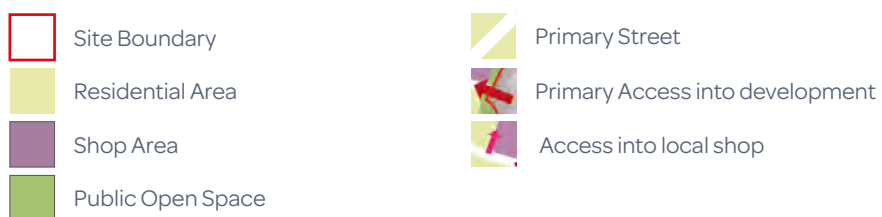
The masterplan offers the opportunity to retain views towards the tower of Holy Trinity church to the North, from the western public open space and other key points on the main route through the north of the site.

The masterplan will include open space (including formal play space), as well as landscaping and a Sustainable Drainage Scheme (SuDS), to manage surface water run-off. The open space has been designed to retain existing trees and hedges where possible within the site.

Formal play space (in accordance with SGCC policy requirements) will be integrated into the open space to allow the preservation of existing trees and hedgerows. These spaces have been positioned to be overlooked and will include a LEAP and a LAP.



Figure 18: Land Use Parameter Plan





# Building Heights

The masterplan has been sensitively designed to respond to the features and topography of the site, surrounding local context, and allow for the creation of character areas within the development.

The Parameter Plan opposite provides a framework for the future reserved matters application to provide building heights in-keeping with the identified character areas.

The residential development is principally proposed to be 2 storey with 2.5 storeys buildings being located in nodal spaces and along the primary street. Key buildings with accentuated scale, form and materiality will be positioned at nodal points within the development to add interest to the streetscene, aid orientation, address corners and terminate local views.

Care will be taken at the Reserved Matters stage to create interest in the streetscene and roofscape by varying ridge and eaves height, building forms and through the use of building/design details such as gables and door canopies to reflect the character from the surrounding area. Single storey garage buildings will also be used across the site to further break up the visual appearance of the development.

The northern part of the site will be limited to 2 stories to create a defined character area and the western edge will be predominantly 2 storey with taller 2.5 storey dwellings featuring more prominently within the core and southern gateway, in order to reflect the character of Wickwar.

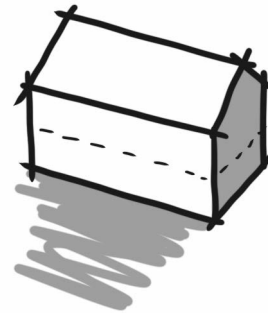


Figure 19: Up to 2 Storey



Figure 20: Up to 2.5 storey development at key locations



Figure 21: Building Heights Parameter Plan



# Access & Movement

## Access Strategy

The access strategy has been informed by the detailed findings of the Transport Assessment taking into account of relevant national and development plan policy and devises up the access strategy for the development.

It is envisaged that the site would be served by two points of vehicular access, from B4060 Sodbury Road. This would take the form of a priority 'give-way' junction, located at the northern and southern ends of the site.

The junction of the proposed southern access point has been designed to incorporate measures to create a gateway into the village and improve accessibility, including:

- proposed traffic calming features at the southern entrance to the village;
- a crossing adjacent to the local shop taking the form of zebra crossing; and
- introduction of a southbound bus stop directly accessible from the southern access.

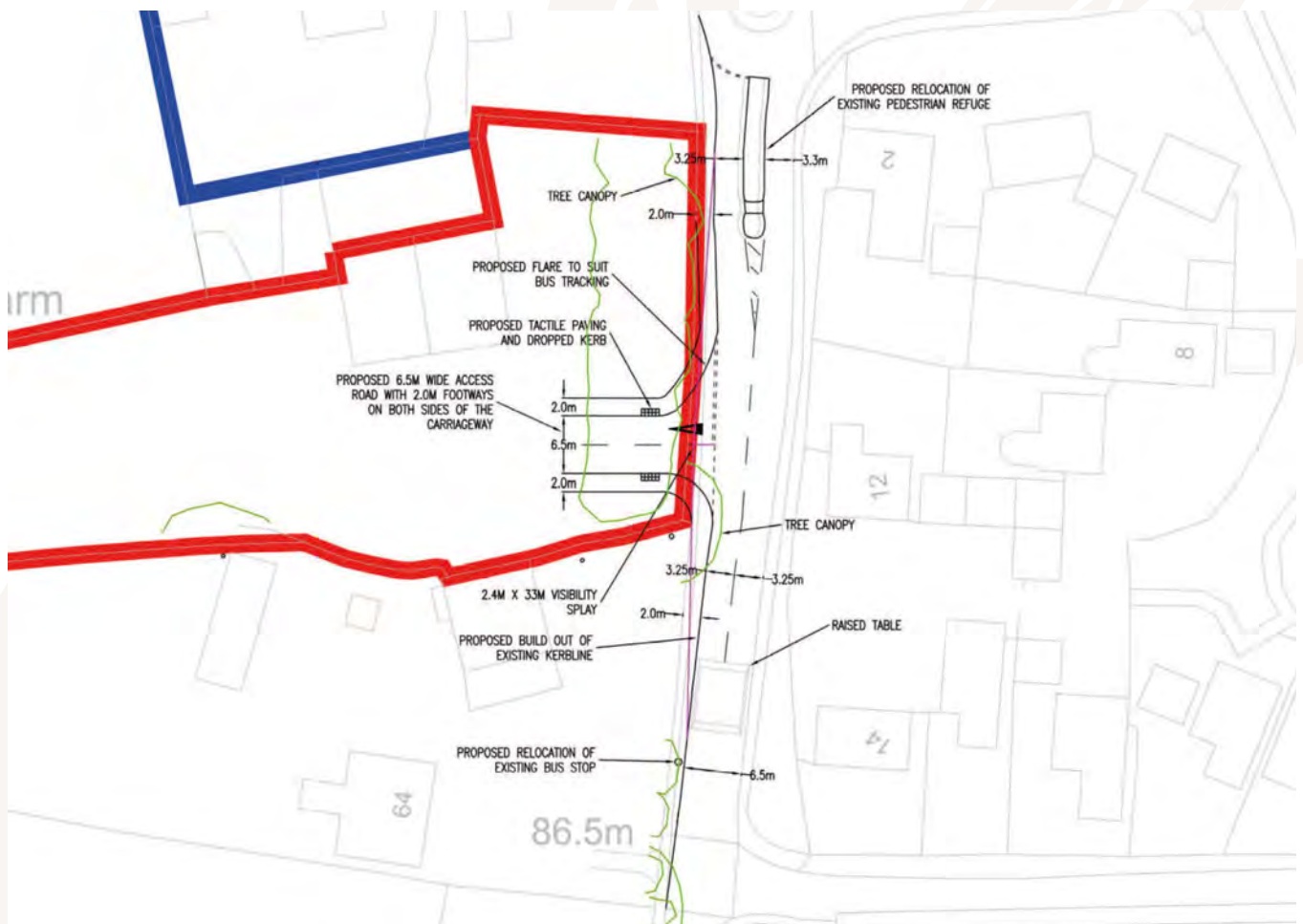


Figure 22: Proposed Northern Access Junction



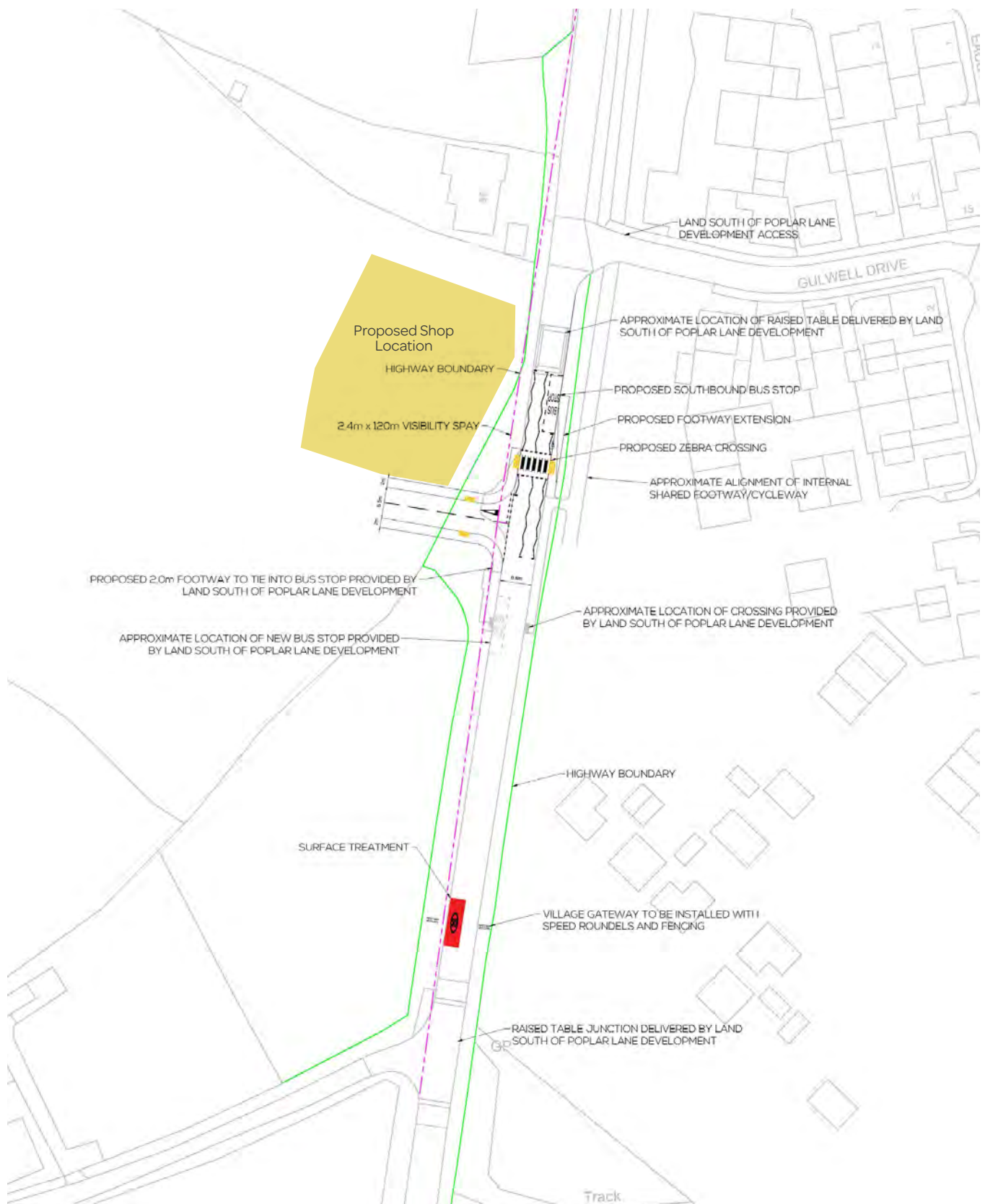


Figure 23: Proposed Southern Access Junction

## Street Hierarchy

The masterplan creates a clear street hierarchy consisting of primary and secondary streets, shared surface, as well as shared drives.

Two access points will be taken from B4066, which will serve the development through a primary street.

Shared surface streets will branch off from the primary streets, and private drives will be fed by shared surface streets.

All of the roads have been designed to encourage low vehicle speeds. This will help to ensure more vulnerable road users feel safe sharing the roads with motorised traffic.

A pedestrian connection is proposed to link to the Public Right of Way near South Farm via the northern site access road and through the adjacent field to the north. This allows the proposal to have a direct link to the PRoW and provides an alternative walking route which avoids travelling through South Farm's farmyard.





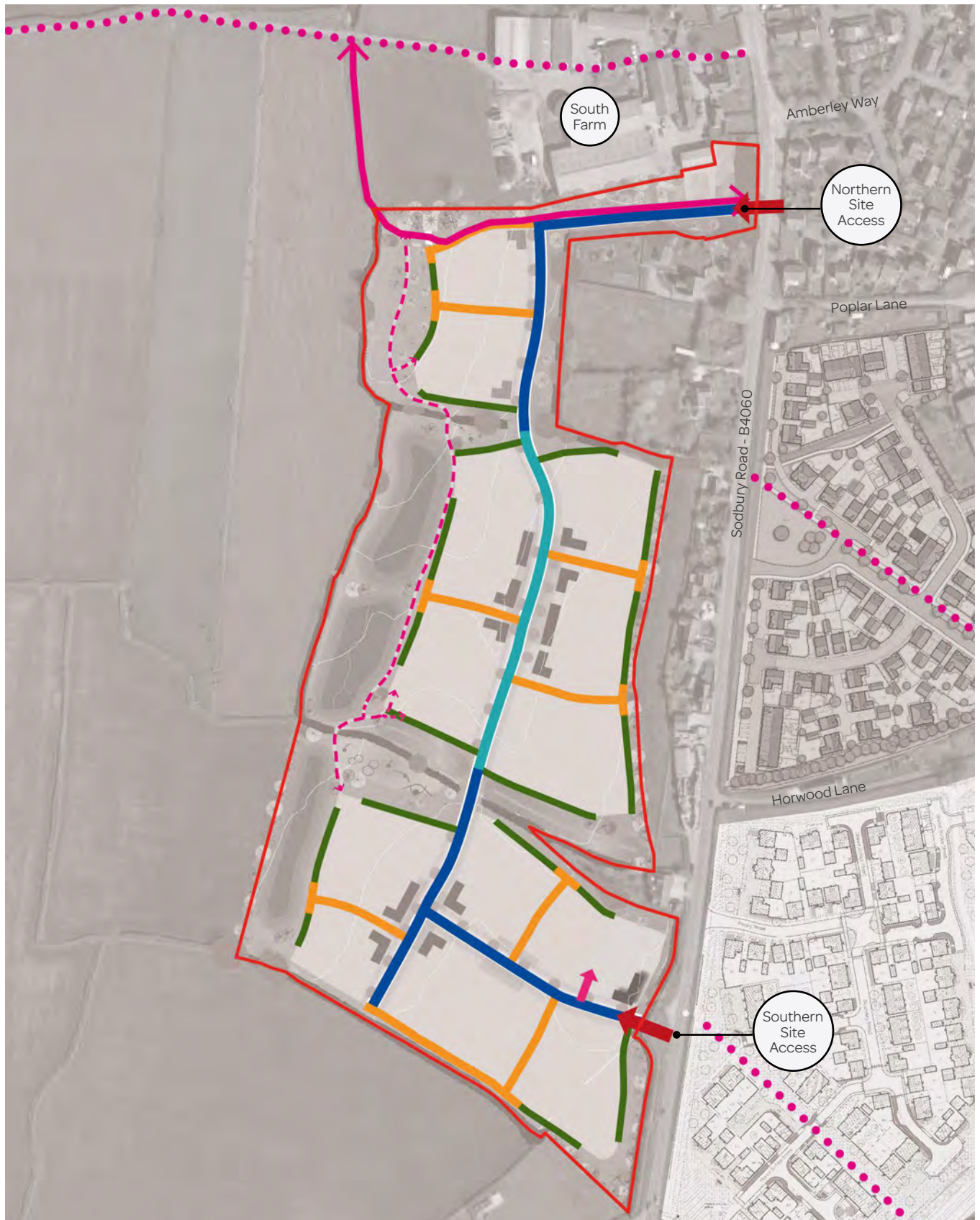


Figure 24: Access & Movement Plan





## Primary Street

The primary street is the main route, forming a spine, through the site, connecting the different areas of the development and open spaces tying the community together. Green verges will provide space for formal street tree planting to create an avenue through the development.

The primary street is defined by a consistent urban form. There will be mixture of 2 - 2.5 storey buildings and will be accessed directly from the street.

Minimal front gardens will feature with hedges and mid size shrubs.

Carriageway and footway surfacing to be tarmac.

Parking will be located on-plot or landscaped frontage parking areas.

### Primary Street Design Principles

Primary Street Corridor	Up to 12.5m
Carriageway Width	Up to 6m
Design Speed	20mph
Footway	2m footpath
Verge	Potential for 2.5m single-sided
Parking Provision	On-plot allocated to the side, On plot to the front of homes. Visitor parking on-street between verges.
Vehicle Swept Path	Refuse Vehicle Passing
Direct Access to Properties	Yes

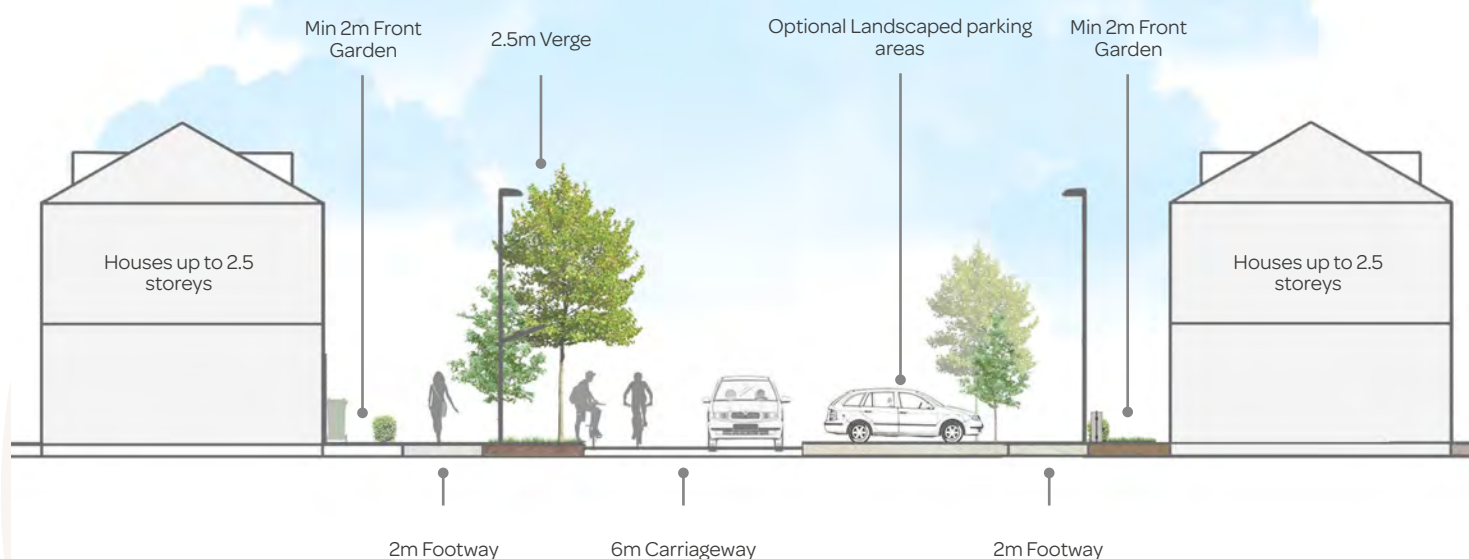


Figure 25: Indicative Street Section - Primary Street

## Primary Street with Rain Garden

Through the central area of the scheme, the character of the primary street will change to incorporate a rain garden into the verge to accommodate .

The primary street is defined by a consistent urban form. There will be mixture of 2 - 2.5 storey buildings and will be accessed directly from the street.

Minimal front gardens will feature with hedges and mid size shrubs .

Carriageway and footway surfacing to be tarmac.

Parking will be located on-plot or landscaped frontage parking areas.

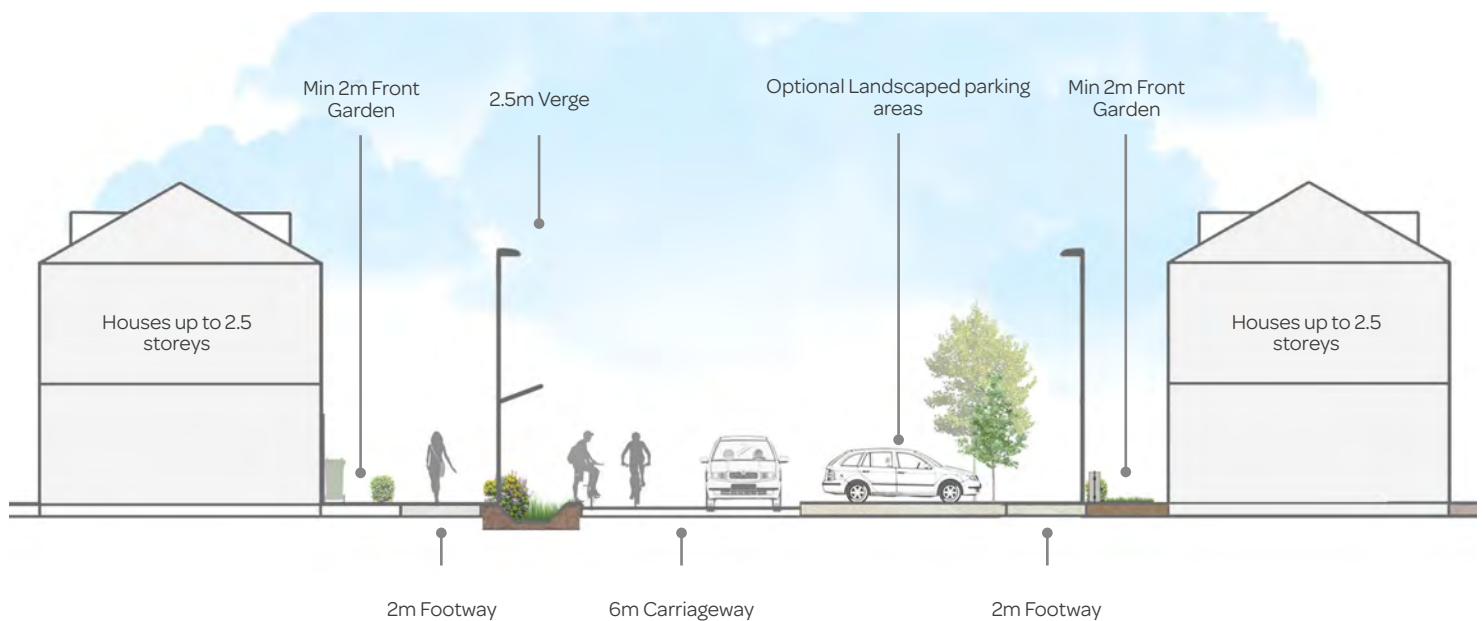


Figure 26: Indicative Street Section - Primary Street with Rain Garden

## Shared Surface Street

Shared surface streets will provide direct access to dwellings and connect to the primary street across the site.

These streets will share vehicular and pedestrian movement, with integrated landscaping finished to a high quality standard. They are narrower and a more enclosed street. Street width is only increased in places to accommodate tracking / swept path assessment for a refuse vehicle.

Parking is accommodated within sections of streetside parking that is intermittent with landscaping.

Surfacing predominantly tarmac with block paving for on-street visitor bays and feature areas to break up tarmac.

### Shared Surface Street Design Principles

Tertiary Street Corridor	Up to 6m
Carriageway Width	Up to 6m (including service zones)
Design Speed	15-20mph
Footway	N/A - Shared Surface
Verge	N/A
Parking Provision	On-plot allocated. Street Visitor Parking
Vehicle Swept Path	Refuse Vehicle Passing
Direct Access to Properties	Yes

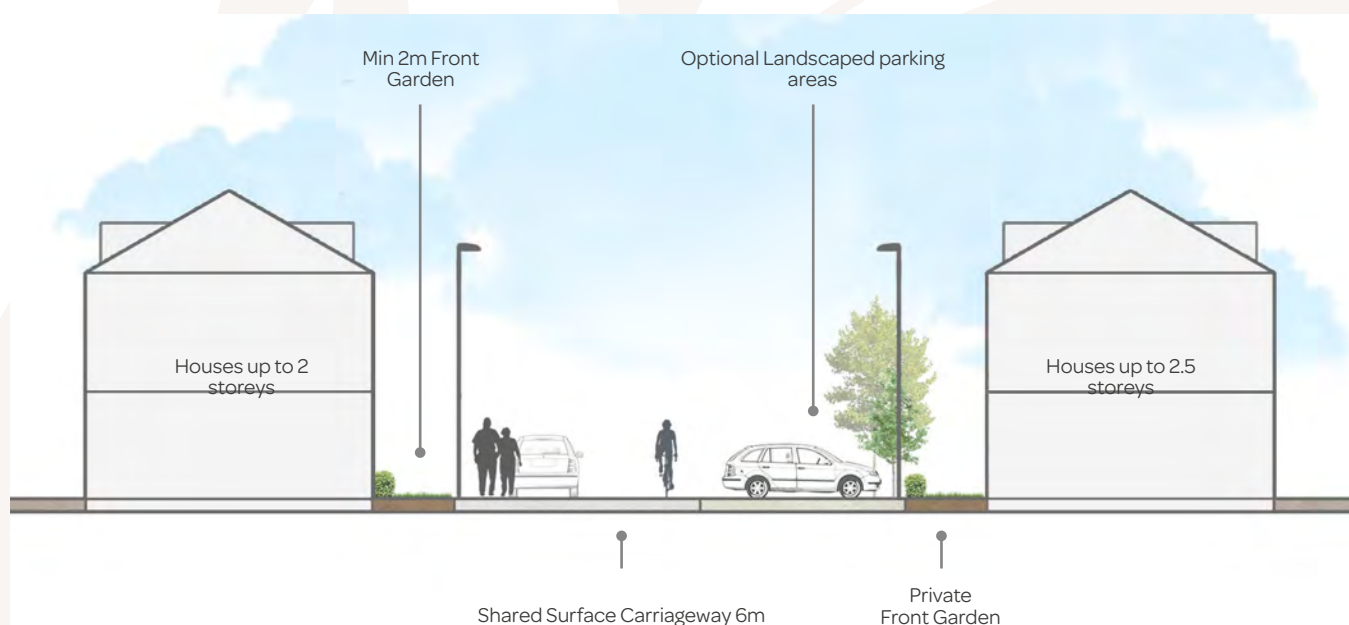


Figure 27: Indicative Street Section - Tertiary Street



Shared Drives

These will serve small groups of houses, typically the edges, such as next to hedgerows or open space. They will have pedestrian priority with varied width and planting to soften the development edge.

Parking will be on plot, with a small number of discreet visitor spaces dispersed, and at times in clusters, on the verge.

Private Drive Design Principles	
Private Drive Street Corridor	Up to 4.8m
Carriageway Width	Up to 4.8m shared surface carriageway
Design Speed	15mph
Footway	N/A
Verge	N/A
Parking Provision	On-plot allocated to the side parking. Visitor parking to side of drive.
Vehicle Swept Path	Privately owned vehicle turning head capacity. Bin Collection Point within 10m of adoptable highway.
Direct Access to Properties	Yes

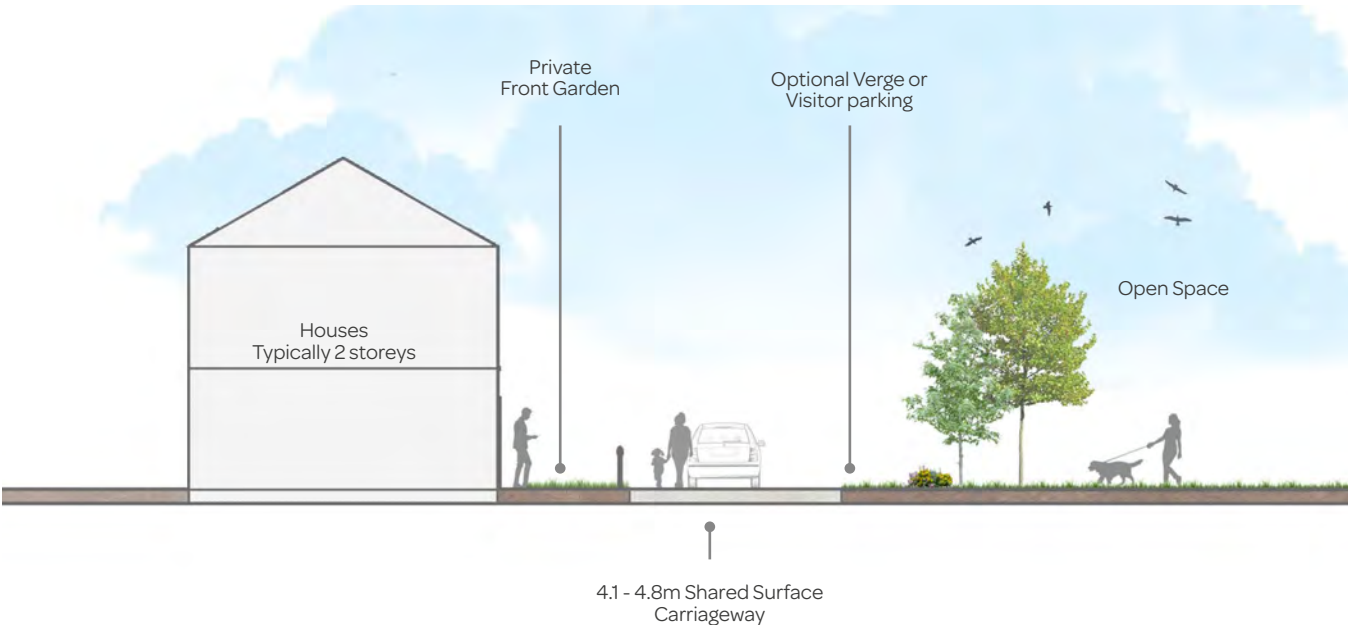


Figure 28: Indicative Street Section - Private Drives



## Car Parking & Cycle Parking

### Car Parking

The parking provision will be determined at the reserved matters stage. However it will be provided in accordance of standards outlined in the South Gloucestershire Council's Residential Parking Standards SPD.

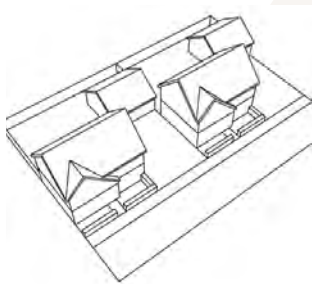
The street hierarchy demonstrates different parking typologies in line with SGC parking guidance. Parking will be provided within side aligned garages with private driveways, at the front of homes, between buildings and on street visitor parking as set out below.

A range of appropriate parking typologies will be used, to ensure that parking provision does not dominate the streetscape.

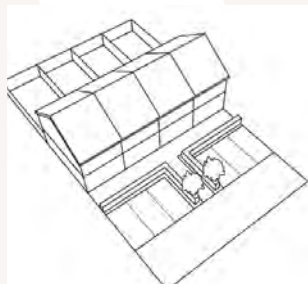
A suitable amount of visitor parking will be provided throughout the scheme and will be in line with policy.

### Cycle Parking

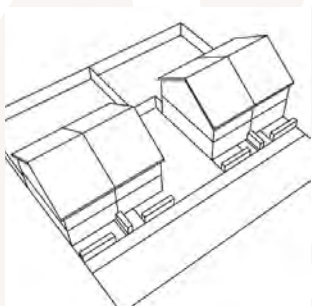
Cycle parking will be provided in accordance with local policy. It is anticipated that in the majority of cases cycle parking can be adequately accommodated within garages (which will be sized accordingly), sheds or other secure storage areas, with sufficient internal space.



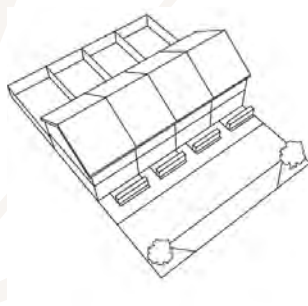
1 Garage on plot between dwellings



2 On plot frontage



3 On plot between dwellings



4 On Street Visitor

## Inclusive Design

Inclusive design aims to create places without barriers that involve people in undue effort, separation or special treatment and enable everyone to go about their daily routines and take part in day-to-day activities independently. The proposed development will be designed to provide barrier-free access for all, with particular regard to the needs of the disabled. Particular consideration has been given to the requirements of the Equality Act 2010.

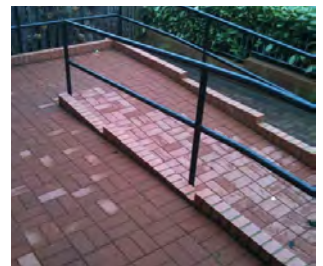
Safe access to buildings will be achieved through the treatment of the site as a pedestrian-friendly environment, where vehicle speeds are limited through traffic calming and the careful design of vehicular routes.

The development will be designed and built in full accordance with the Building Regulations which set out technical standards for the quality / performance of buildings. Part M of the Building Regulations concerns 'Access' and ensures that the design of buildings does not preclude access for the disabled. Where necessary, compliant ramps will be provided externally in addition to steps where changes in levels demand.

## Emergency & Large Vehicle Access

Appropriate tracking / swept path analysis has been undertaken to ensure that the proposed streets can operate safely for all vehicles including emergency service vehicles, refuse lorries, HGVs, vans and delivery vehicles.

Waste collection and recycling points will be designed close to the edge of the carriageway and as close as possible to the publicly-maintained road for convenient collection whilst minimising visual impact.



# Character and Appearance

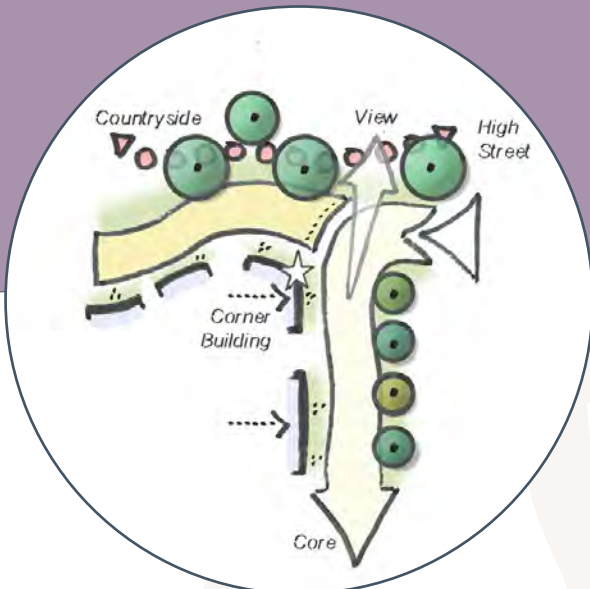
The appearance of the proposed development will be informed by the context character appraisal identified in the assessment section of this Design and Access Statement, which learns from Wickwar; its historic core and recent developments, closely located to the site.

The development's character will use a carefully selected material and colour palette directly influenced from Wickwar.

Precise use of detail and materials will be subject to future reserved matters applications, but will be underpinned by the character appraisal set out within this DAS.

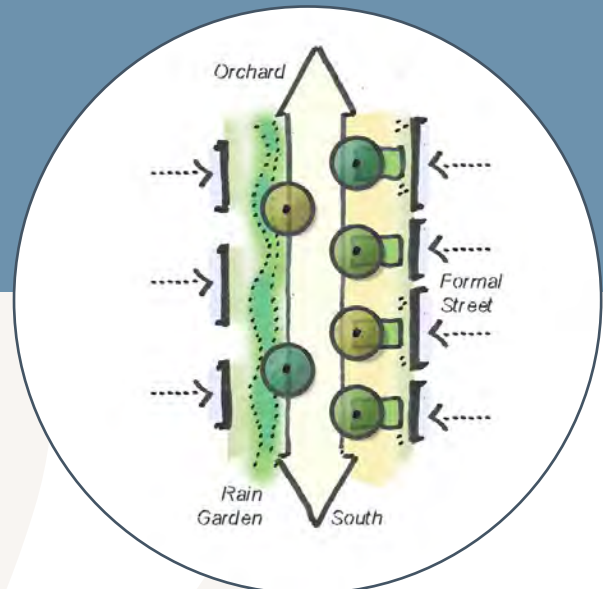
## Orchard View

The threshold between the development and the High Street of Wickwar



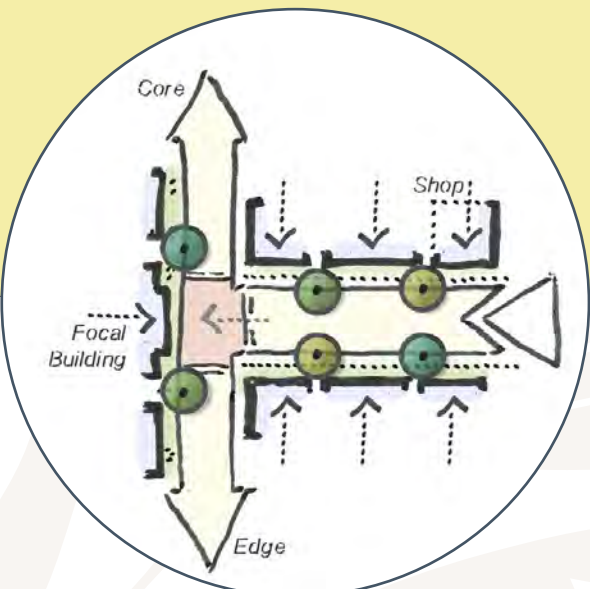
## Development Core

The heart of the development, punctuated by a series of key nodal spaces



## Southern Gateway

A new entrance gateway into Wickwar and the development from Wickwar Road - B4060



## Green Edge

A landscaped edge onto open space

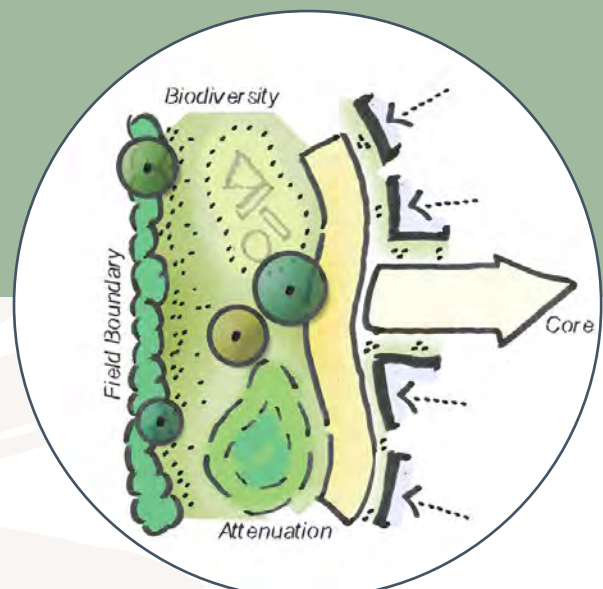
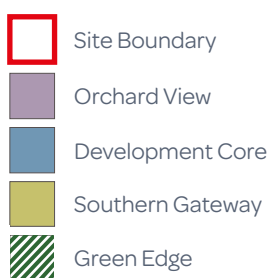






Figure 29: Character Areas Plan



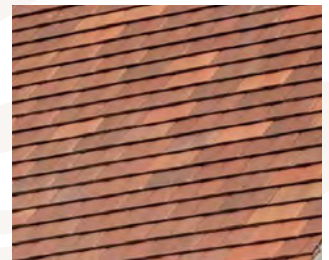
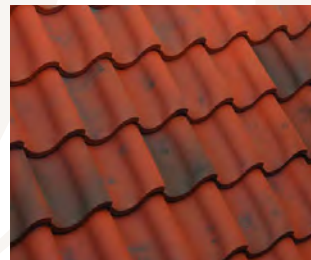
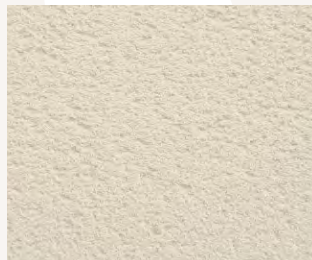
## Orchard View

This is the area of the development is closest to the listed building at South Farm. As such this area will be of a lower density to respect the listed buildings and to provide a softened entrance leading to the development core.

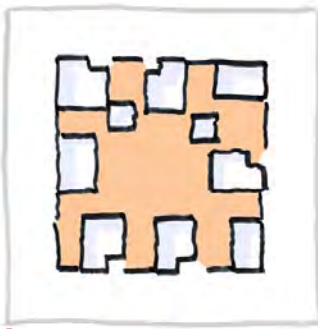
- Predominantly detached and semi-detached houses.
- 2 Storey
- Narrow front gardens
- Non-fruit bearing street trees
- Orchard proposed within Public Open Space
- Continuous building line along primary street
- Informal, lower density edge fronting the orchard green

## Materials

- Natural stone to key plots
- Reconstituted stone elsewhere
- Red/brown tile and composite slate roofing



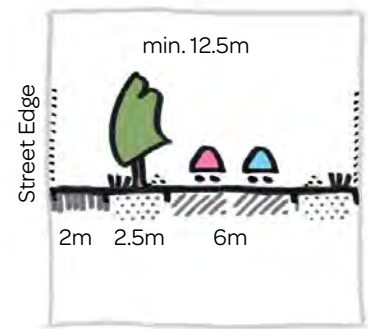




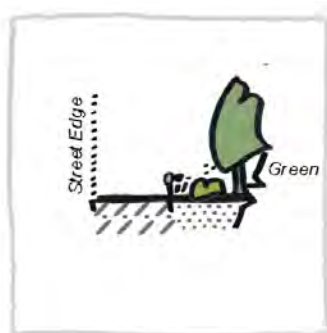
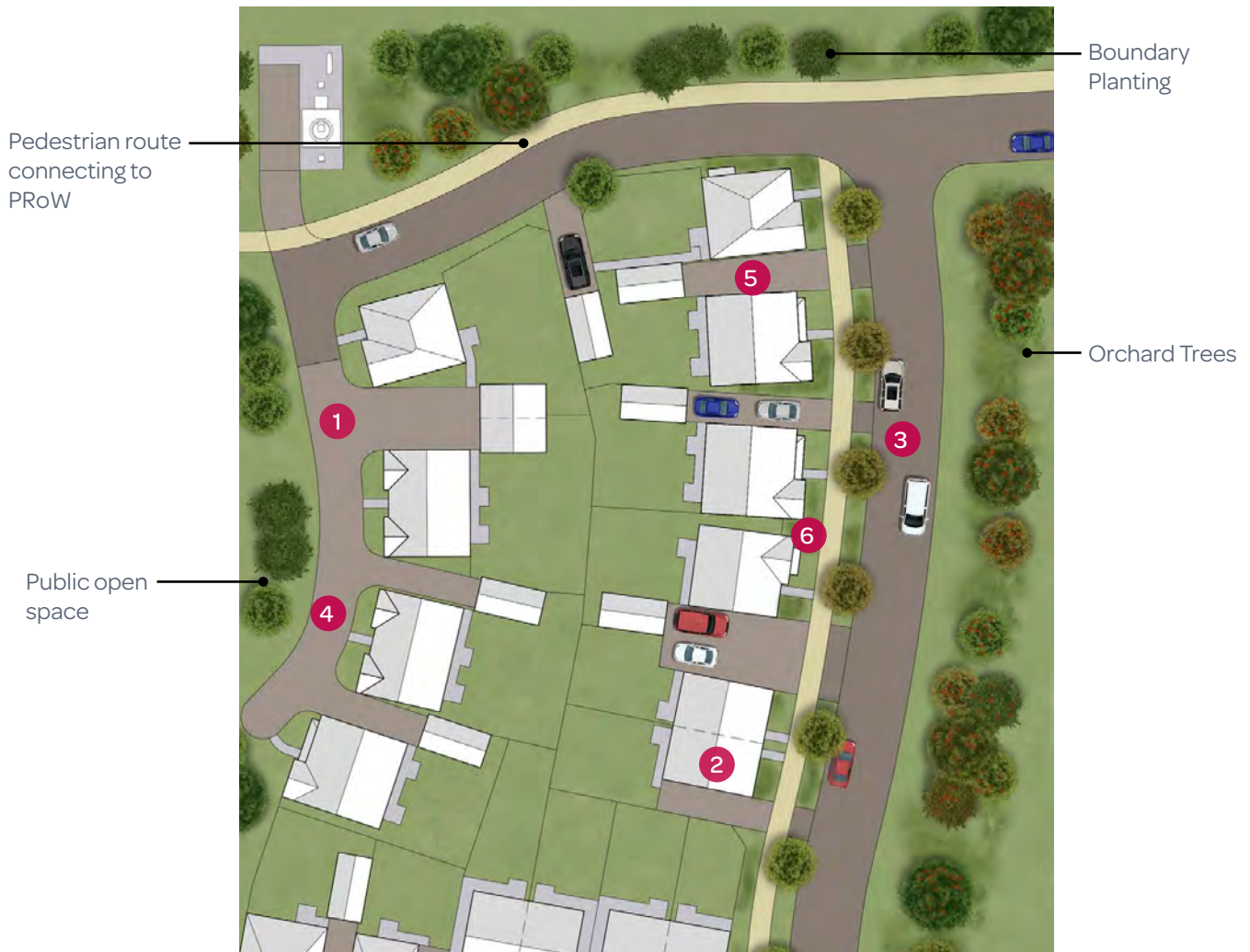
1 Low density along orchard green edge



2 Moderate density along primary street



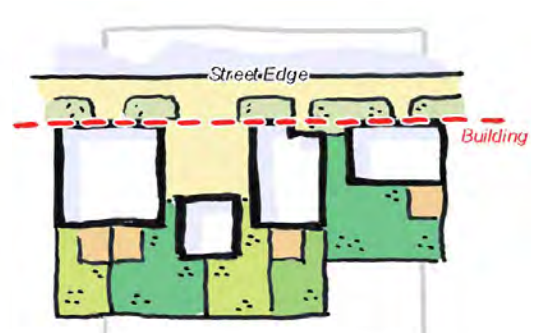
3 Typical primary street character



4 Typical green edge, shared surface character



5 On plot to the side parking used primarily throughout Orchard Green area.



6 Continuous building line along primary street



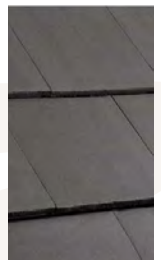
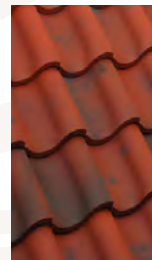
## Development Core

This is the main body of the residential area along the primary street. This area will be the higher density core, with a consistent form. Development core will comprise of:

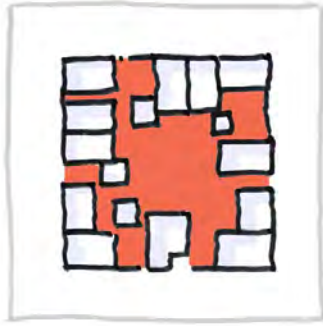
- Predominantly semi-detached and terraces along the primary street with some detached dwellings
- Mix of 2 and 2.5 storey
- Varied streetscape and roofline
- Rain garden on primary street incorporating street trees
- A series of key nodal spaces identified by a change in materiality and/or surfacing such as corners and junctions
- A mix of frontage parking or on plot, to the side parking

## Materials

- Predominantly reconstituted stone with some render
- Predominantly red roof tiles with grey tiles to create variety



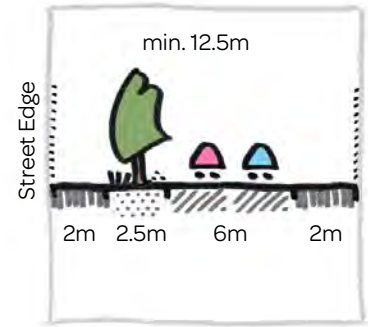




1 Higher Density Core Area



2 On plot to the side parking used on secondary streets and in conjunction with front-of-plot parking



3 Typical primary street character



Shared surface street

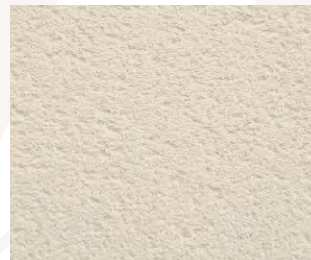
## Southern Gateway

This is the main body of the residential area along the primary street in the south of the site and forms the southern gateway to Wickwar with the development on the opposite side of Sodbury Road. This area will be the higher density core, with a consistent form and will comprise of:

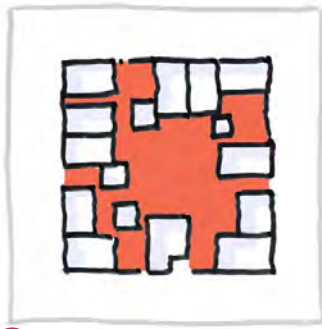
- Space for a shop provided adjacent to Sodbury Road
- Semi-detached, terraces and a lower proportion of detached dwellings
- Mix of 2 and 2.5 storey
- Tree lined verges to primary street
- Narrow front gardens
- Formal streetscape
- A mix of frontage parking or on plot, to the side parking

## Materials

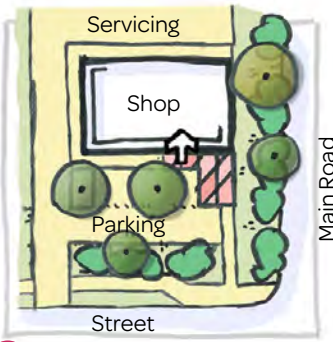
- Predominantly brick with render to key buildings
- Predominantly red roof tiles



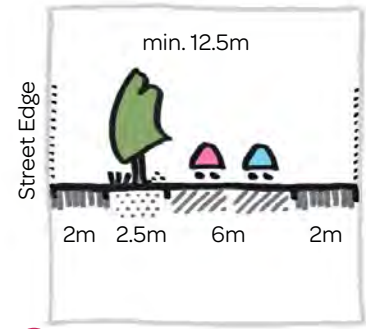




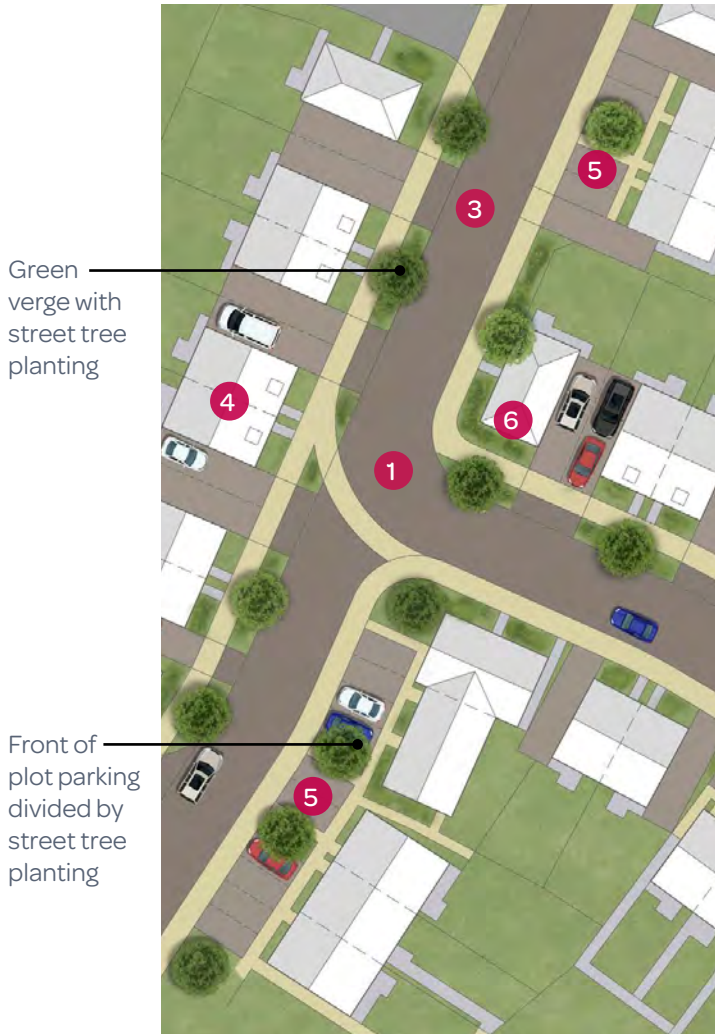
1 Higher Density Core Area



2 Gateway Shop

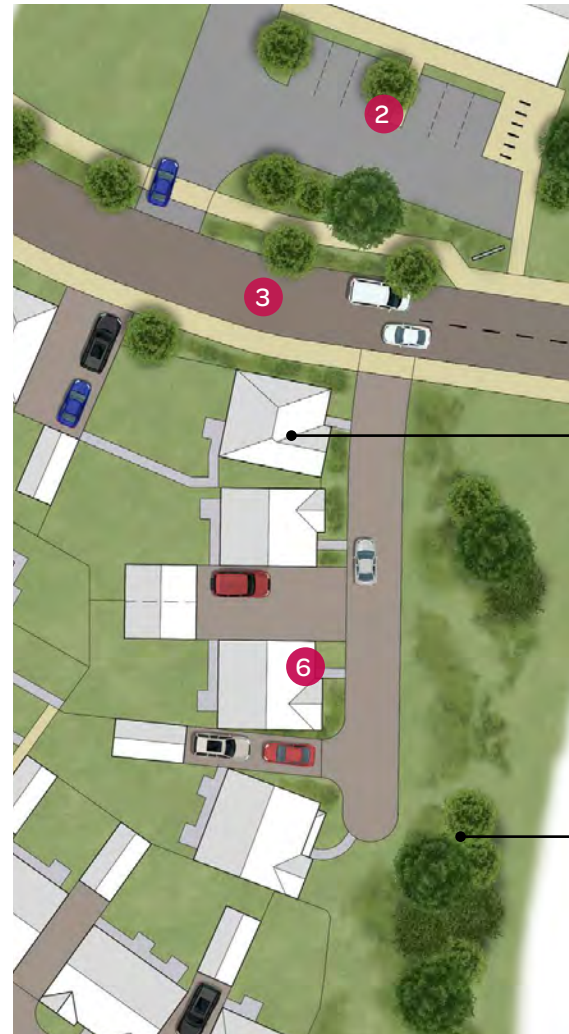


3 Typical primary street character



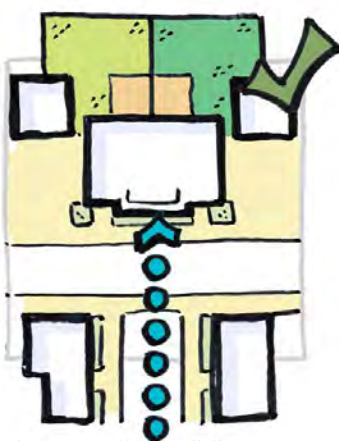
Green verge with street tree planting

Front of plot parking divided by street tree planting

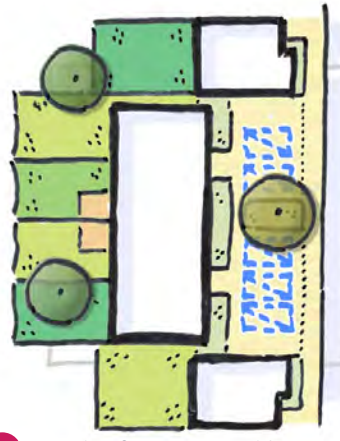


Corner dwelling marks southern gateway

Boundary planting



4 Vista onto key building



5 On plot frontage parking along primary streets



6 On plot to the side parking



## Green Edge

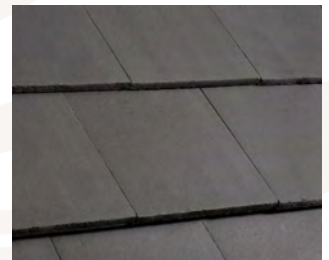
Lower density edge fronting onto wetland and open spaces, providing a soft countryside edge and natural surveillance to areas of public open space.

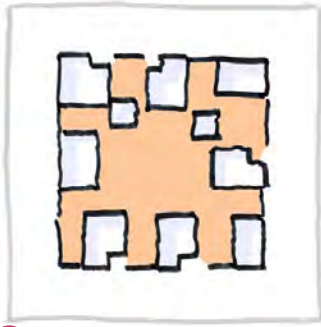
Informal pedestrian routes and areas of play will run through the open space next to areas of new and enhanced tree planting, alongside dry and semi-wet natural drainage to support a series of wildlife species. It comprises:

- More informal staggered development edge, with a varied building line whereby homes step forward and back from the green space.
- Detached and semi-detached properties
- Shared drives and pedestrian priority routes
- Mostly two storey development.
- Multifunctional open spaces including play areas and attenuation

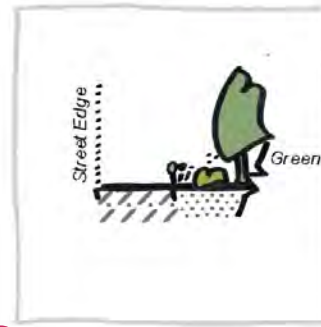
## Materials

- Mix of brick, reconstituted stone and rendered elevations.





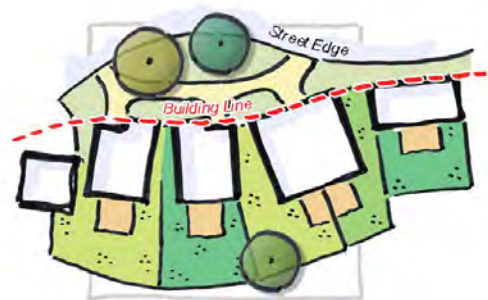
1 Low density along orchard green edge



2 Typical green edge, shared surface character



3 Attenuation basin provided within green corridor



4 Varying/informal building line



## Demonstration

This application is submitted in outline, with all matters reserved except access, so the exact appearance will be for future reserved matters applications. However, this section provides a guiding vision for the character and appearance of the scheme that will respond to the local context and features of the site.

The illustrative layout demonstrates how the scheme of 180 homes can be brought forward in line with the Framework Masterplan, Parameters and proposed character areas to sympathetically reflect the existing pattern of residential development in this part of the village.

- 1 Primary Access Points
- 2 Main Spine Road
- 3 Gateway Shop
- 4 Play Areas
- 5 Allotments
- 6 Orchard Planting
- 7 Attenuation Basins
- 8 Swales
- 9 New Boundary Planting
- 10 Retained Hedgerows
- 11 Raingarden

-  Site Boundary (7.89 ha/19.49 ac)
-  Residential Development (4.41 ha/10.91 ac)
-  Potential shop extents (0.15 ha)
-  Open Space including Informal Recreational and Natural & semi-natural urban green space (3.24 ha)
-  Allotments (0.09 ha)
-  PROW (public rights of way)
-  Proposed attenuation basin
-  Areas of boundary to be bolstered with additional Planting
-  Existing Trees and Hedgerow
-  Proposed Pumping Station
-  Proposed Rain Garden

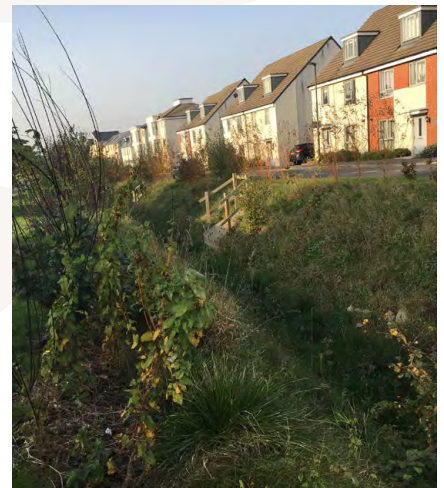
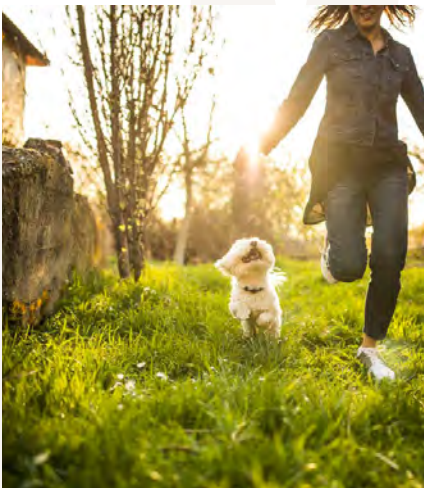






Figure 30: Demonstration Layout



## Proposed Shop

A new convenience store for Wickwar. Located within the Southern Gateway character area it will serve both new and existing residents. It will be up to 1,000m<sup>2</sup>, a size able to cater for day to day conveniences.

It will be located on the doorstep of new and recently built homes and around 1km from the High Street. The whole village will be within 20 minute walk. The store will be highly accessible, convenient and a valuable amenity for the wider community.

The store itself will be in a visually prominent position on Sodbury Road near a new junction and not far from where people arrive at the village by car from the south. It's corner location will be a community landmark and an identifiable point within Wickwar's built environment. It will be an important node, arrival image and part of resident's day to day lives for the new homes.





# Secured by Design

The design of the scheme can help to reduce opportunities for crime and enhance people's feeling of security within their home and neighbourhood.

The layout of dwellings should ensure that homes and places are easy to move around and safe and secure as set out in Secured by Design guidance. Layouts need to ensure natural surveillance from buildings to public spaces, encourage community interaction, engagement and participation and environmental control.

A series of contributing factors help to encourage a safe urban environment such as well lit streets.

Factors associated with the built form can promote natural surveillance such as:

- Maximising perimeter blocks to ensure spaces are overlooked by dwellings.
- Clear delineation between public and private realm.

- Side and rear boundaries to have more robust defensible boundaries such as walls or fencing with a minimum height of 1.8m.
- Providing an active frontage throughout, ensuring dwellings overlook the public realm.
- Clear signage to allow for emergency services to identify plots.
- Parking within garages or on hardstanding to the side or front of plots.
- Street Lighting.
- Ensuring new planting to not impede on natural surveillance.

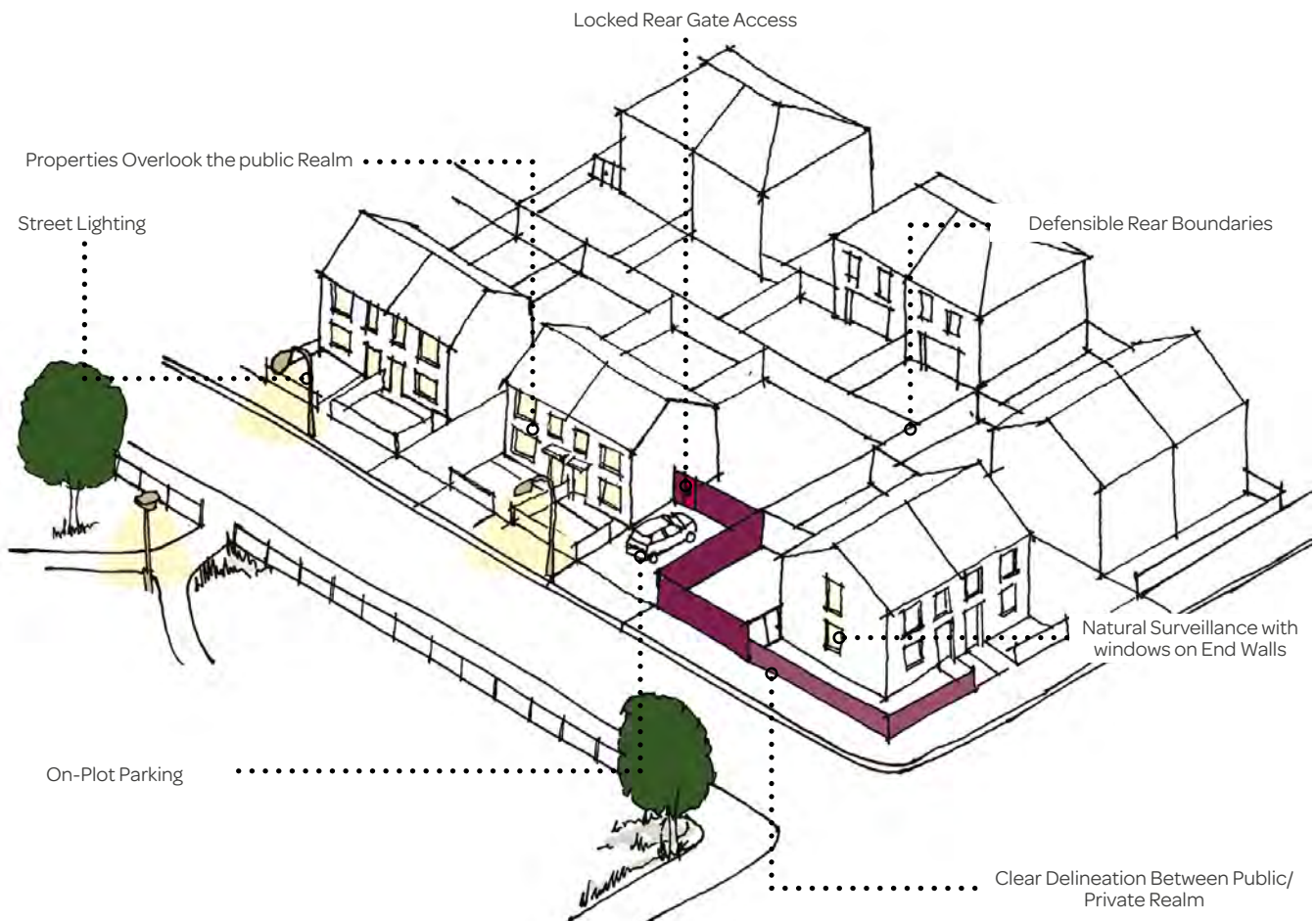


Figure 31: Illustration of Safer By Design Principles

# Landscape

## Landscape Strategy

A total of 3.25 ha of the site (approx 41%) will be provided as green infrastructure and public open space. This includes 2.58ha of public open space to reflect South Gloucestershire policy requirements.

The existing vegetation will be retained and enhanced, wherever possible. The masterplan will deliver additional planting within the scheme, particularly along the boundaries to help to provide a sense of enclosure. Street tree planting will also be provided within the masterplan to help soften the street scene.

Hedgerow corridors have been retained wherever possible and incorporated into the masterplan proposals.

The western green corridor will aid new homes to naturally integrate with the existing rural landscape setting.

The landscape strategy integrates play and community spaces throughout the site to create opportunities for social interaction. At the northern gateway an area of allotments has been provided so that it is easily accessible for all residents of the village, a LAP (Local Area for Play) is provided in the north of the site to provide accessible, doorstep play and a Local Equipped Area for Play (LEAP) is provided in the south of the site. Both of these play facilities will be designed to national guidance on play spaces (Fields in Trust).

Category of Open Space	Quantity Standard per 1000 population	Policy compliant areas required for the site	Proposed Quantum	Compliant to Policy
Informal Recreational Open Space	1.4 ha	0.60 ha	2.31 ha	Yes * (+ 1.06 ha)
Natural & semi-natural urban green space	1.5 ha	0.65 ha		*Informal recreational open space and natural & Semi-Natural green space combined together for parameter plan
Outdoor Sports Facilities	1.6 ha	0.69 ha	0 ha	No. An off-site contribution to sports provision will be required for incorporation within a S106 agreement
Provision for Children and Young People	0.25 ha	0.11 ha	0.11 ha	Yes - LAP and LEAP
Allotments	0.2 ha	0.09 ha	0.09 ha	Yes
<b>TOTAL</b>	<b>4.95 ha</b>	<b>2.14 ha</b>	<b>2.58 ha</b>	

NOTE: Attenuation basins are excluded from POS calculations









## Landscape Character

The masterplan creates an enjoyable, safe and attractive place, the proposed landscape setting plays a vital role in shaping these attributes.

The proposal delivers a new high quality, landscape corridor along its western boundary providing new landscaped sustainable urban drainage systems, retained planting as well as new and enhanced planting. A new Local Area for Play (LAP) and Local Equipped Area for Play (LEAP) will be provided within the proposal.

Outside of the new landscape corridor, existing trees and hedgerow will be retained and enhanced where possible with additional planting.

New allotments are proposed to the north of the site, which will allow the community to grow their own produce and will help to positively contribute to health and well-being.

The masterplan is bisected by two hedgerows running east to west. These form the masterplans emphasis for green infrastructure and green outlook for new homes. These green corridors separate the masterplan into three parcels.

A new SuDS Swale corridor will separate the central and southern parcels, the swale will be planted and set in the landscape providing a new enhanced feature within the masterplan.

The primary street will feature a 2.5m verge with a raingarden and street tree planting within the Development Core area, as well as street tree and low level planting within the Southern Gateway and Orchard Gateway areas.

The overarching landscape strategy is designed in such a way to encourage biodiversity and wildlife to use the site through retention enhancement and the introduction of new habitats.







Link to PRow

Bolstered Tree Planting

Orchard Trees

New Allotments

Amberley Way

Poplar Lane

Sodbury Road - B4060

Horwood Lane

New High Quality Wet Habitat

Rainwater incorporated within verge

New SuDS Swale Corridor

Verge incorporating tree planting

Retained & Enhanced Trees & Hedges

Strong Planted Frontage on Sodbury Road



## Allotments

To celebrate the historic land uses within the northern part of the site and respect the neighbouring Grade II Listed South Farm, an area of allotments and community orchard will be provided. This location is also easily accessible for all residents of Wickwar.

The 0.09 ha allotment area can incorporate up to ten quarter size allotment plots (60-70m<sup>2</sup>). This reflects modern lifestyles, as smaller plots are more accessible to new starters, couples and smaller families. The illustrative plan shows that an area for storage sheds and a central water supply is also provided alongside paths between the plots. The exact layout will be determined through detailed design at the reserved matters stage.

Alongside the allotments, an area of parking will be provided to allow for easy access for deliveries. The illustrative plan shows four parking spaces and a turning area.

The wider area around the allotments will be landscaped with appropriate fruit trees to reflect the historic character of the area and provide an informal community orchard for people and wildlife to enjoy.







Figure 33: Illustrative Allotment Design



# Drainage & Water Management

The masterplan will reduce the amount of permeable surfaces within the site and thus increase surface water run off.

The masterplan's drainage strategy includes a series of attenuation basins distributed through the lower lying western part of the site, to mitigate against flood risk from the development and accounts for an allowance of 40% for climate change. The basin is proposed to discharge surface water into the minor watercourse at the west of the site, via an adoptable surface water flow control chamber limiting discharge to a maximum discharge rate of 25l/s.

These basins are designed to be a range of depths between 1m and 1.5m, with a maximum gradient of 1:3 for slopes. This will allow them to be accessible amenity space.

A central swale is also provided, running east to west, which will manage surface water flows and discharge water into the attenuation basin.

Along the central length of the primary street, rain gardens will be incorporated into the verge to manage surface water flows and provide an attractive and varied streetscape.

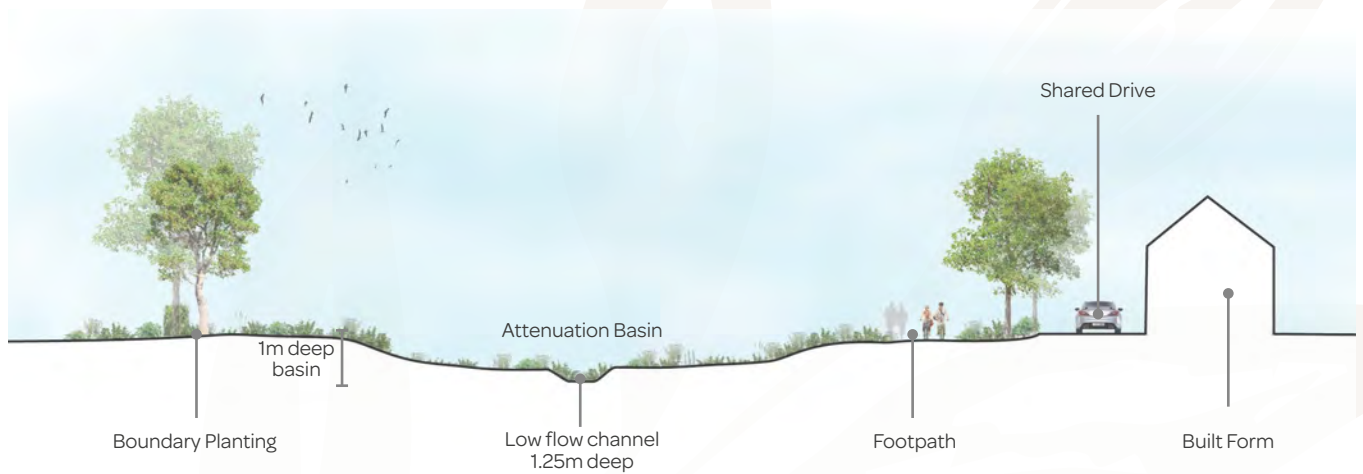


Figure 34: Illustrative Cross Section of Attenuation Features within Open Space



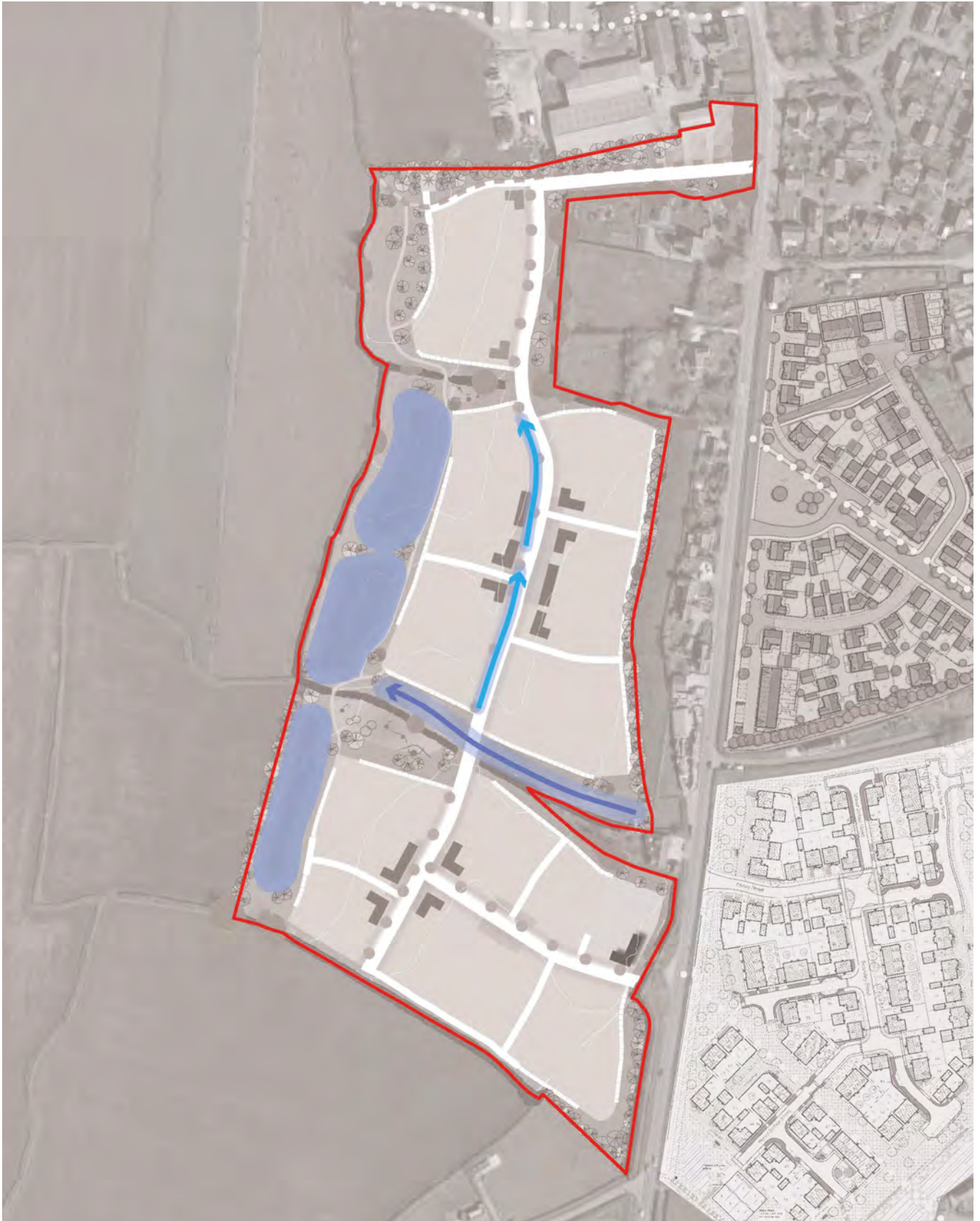
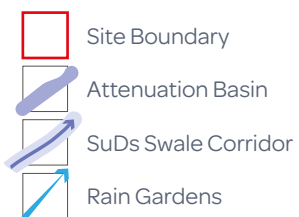


Figure 35: Drainage Strategy Plan





## Ecology

The proposal mitigates against adverse ecological impact to the site. This is achieved with species rich planting including meadow grassland, to help create high quality habitats to encourage wildlife. These will help to create foraging habitat for bat and bird species as well as a new pleasant environment for new and existing residents.

Existing trees and hedgerow are retained wherever possible and appropriate buffers are provided where hedgerows are ecologically sensitive.

The masterplan responds to potential bat populations that use the hedgerows for foraging and/or nesting by providing a wide new green corridor along its western edge to preserve these habitats.

Green space shall host bug hotels to further increase the level of biodiversity within the site and have educational value for younger people with associated educational boards demonstrating the importance of wildlife and biodiversity.

Additional tree planting will be planted appropriately within the scheme to help deliver a new ecologically responsive masterplan. This will be through both urban street tree planting and within open green space, a mixture of tree planting and orchards.





The new swale, rain gardens and attenuation basins will provide added ecological value and support new wetland habitats.

Bat and Bird boxes will also be incorporated into the masterplan proposals, exact locations will be determined at Reserved Matters stage of the application.









# Summary



## Conclusion

The masterplan demonstrates how land at Wickwar could deliver new homes in a sustainable location.

The masterplan proposes up to 180 new homes, that range in type tenure and size, and reflect local housing need.

Two new access points will be taken from the B4066 and will serve the main body of the development.

The development will reflect and respect the local character of Wickwar in terms of design and materials.

The scheme meets Public Open Space standards as outline by policy CS24, providing a new LEAP, LAP, accessible open space, allotments and offsite contributions for outdoor sports facilities.

The drainage strategy mitigates against adverse impact as a result of the scheme and takes into consideration 40% for climate change.

Overall, the site is situated in a sustainable location, with access to local facilities and public transport. The scheme will positively contribute to Wickwar by introducing a new village shop to serve the wider community.







**Turley**